

PUBLIC REVIEW DRAFT

September 2020

UDA

DOWNTOWN RIVERHEAD PATTERN BOOK

Riverhead, New York

This page is intentionally blank.

DOWNTOWN RIVERHEAD PATTERN BOOK

PREPARED FOR

Town of Riverhead

FUNDED BY

Town of Riverhead

TOWN SUPERVISOR

Yvette Aguiar

TOWN COUNCIL

Frank Beyrodt

Jodi Giglio

Timothy Hubbard

Catherine Kent



TABLE OF CONTENTS

Executive Summary	IV	OBJECTIVE DESIGN STANDARDS FOR PRIVATE DEVELOPMENT	
INTRODUCTION		Overview	59
Study Area Boundary/Concurrent Studies	3	Lot Development Standards	61
Brief History of Downtown	4	Architectural Principles	65
Listening and Observing	8	Materials	69
		Architectural Styles	71
POLICY RECOMMENDATIONS		APPENDICES	
Overview	15	Appendix A: Glossary	77
Peconic River Flooding	17	Appendix B: SWOT Analysis	79
Parking	19	Appendix C: Online Surveys	83
Proposed Mix of Uses	21	Appendix D: Online Surveys	97
Maximum Build Out	23	Appendix E: Ranking Challenges	109
Transportation	25	Appendix F: References and Resources	123
Special Events	27	Appendix G: Reconnaissance	125
Transfer of Development Rights	29		
Sustainability	31		
GUIDELINES FOR PUBLIC AREAS			
Overview	35		
Vehicular Circulation	37		
Pedestrian Circulation	39		
Bicycle Circulation	41		
Pedestrian-Friendly Streets	43		
Pedestrian-Friendly Passages	45		
Public Safety	47		
Parks and Open Space	49		
Town Square	51		
Signage	55		

EXECUTIVE SUMMARY

The Downtown Riverhead Pattern Book is intended to provide direction for policies and projects proposed within the Main Street (DC-1) Zoning Use District. The process of listening to residents and stakeholders informed an ideal vision for Downtown Riverhead, and the parameters that follow reflect the desired character for the policies and the public and private realms that came out of that public process. The principal goal is to preserve the unique character of Riverhead while also accommodating redevelopment projects of varying scales. These parameters therefore cover the proper siting and massing of buildings, the enhancement of the public realm, and the issues of resiliency and adaptability in terms of public policy. This Pattern Book is intended to establish a foundation of guiding principles for new policies and projects proposed in the DC-1 boundary and will be adopted by the Town as part of a comprehensive review process.

Policy Recommendations

Based on extensive community outreach, some over-arching issues emerged that are broader than what is typically covered in a Pattern Book. These issues include future-proofing and resiliency in terms of recurring flooding events, future parking and transit needs, desired development densities, special events planning, and more. The recommendations included in this section will be used to illustrate existing and inevitable challenges in order to solicit solutions to ultimately be enacted through the local zoning ordinance.

Guidelines for Public Areas

The management of consistent site elements is crucial to the design of an aesthetically contextual and appropriately scaled public realm. To enhance the safety and experiences of pedestrians, cyclists, and motorists in Downtown Riverhead's parks, streets, and passages requires physical improvements. The guidelines in this section will be used to illustrate possible improvements to the public realm that will be implemented through the Town's zoning ordinance.

Objective Design Standards for Private Development

This section of the Pattern Book is intended to aid in the visualization of the massing, siting, and stylistic parameters prescribed within the zoning districts. The principle goal is to preserve the unique character of Downtown Riverhead while also accommodating redevelopment at many scales. These design standards specifically define required lot dimensions, setbacks, building height, lot coverage, development densities, and architectural style in order to regulate individual architectural form, in turn creating the consistent atmosphere throughout the downtown area that the community desires.



This page is intentionally blank.



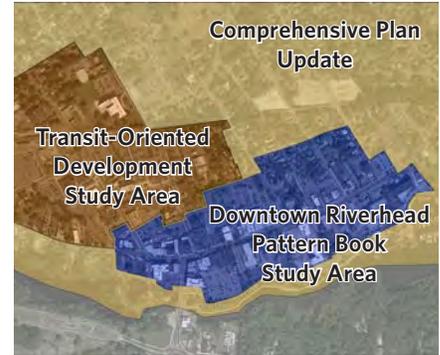
INTRODUCTION

STUDY AREA BOUNDARY/CONCURRENT STUDIES

DC-1 ZONING USE DISTRICT

The Study Area Boundary was taken from the Downtown Center 1: Main Street (DC-1) Zoning Use District adopted in 2004 and adapted to include the adjacent residential neighborhood to the north of Main Street. The intent of the DC-1 Zoning Use District is to “allow, maintain, and foster a traditional downtown character along Main Street, with a pedestrian-friendly streetscape, active ground-floor uses, a twenty-four-hour presence from upper-story residential, with a compact, walkable scale” (§ 301-140).

At the time of publication, two other studies are being conducted in the Town of Riverhead: a Transit-Oriented Development Study centered around the Long Island Rail Road Station just outside of the Downtown Riverhead Pattern Book Study Area boundary and a Comprehensive Plan Update for the entirety of the Town of Riverhead.



Concurrent Study Boundaries



The Downtown Riverhead Pattern Book Study Area boundary encompasses slightly more residences than the DC-1 Zoning Use District boundary.

BRIEF HISTORY OF DOWNTOWN

Located at the point of divergence between Long Island's North and South Forks, Downtown Riverhead is the County Seat and acts as a natural hub for transportation, commerce, and recreation in the East End. The Peconic River that splices the island forms the Town's southern border and is provides a scenic backdrop for Riverhead's historical downtown district. Main Street runs east to west, parallel to the river, with cross streets Peconic Avenue and Roanoke Avenue forming a major intersection at the easternmost crossing point between the Forks. This highly-advantageous location is critical to the history of development along Main Street and throughout the Downtown.

The buildings standing today in Downtown Riverhead tell the story of American retail evolution. For the latter half of the nineteenth century, when Downtown Riverhead was first developed, commercial buildings were sited along the Main Street corridor and featured tall retail floors with open storefronts to attract pedestrians. There was little to no setback off the sidewalk and stores were relatively small and shallow in order to show all the merchandise to potential consumers walking past. These retail floors were often accompanied by residential units above, both for the shopkeepers and other residents. This concentration of people helped to support a vibrant local retail scene. Churches and large single-family houses were also built at this time and generally featured deep setbacks and ornate architectural features. These larger residences were mostly kept off of Main Street, but were within walking distance to the commercial amenities.

The turn of the twentieth century was marked by economic upheavals and architectural style evolution. To save on construction costs, commercial buildings were often narrower with 1-2 stories and made with slightly cheaper materials. These buildings, like the Lee Building and Digger's, acted as infill between the existing commercial and institutional buildings, extending farther from the highly-trafficked intersection at Main Street, Roanoke Avenue, and Peconic Avenue.

The popularization of the car around the 1950's drastically impacted the retail landscape. Department stores with large footprints were favored over small storefronts. Many residential buildings were demolished to make way for expansive surface parking lots, particularly along E 2nd Street, where today only two historical residences remain. The local "big-box" retailers like Swezey's and Woolworth dominated downtown, focusing retail inward and discouraging pedestrian activity. National big-box chains, though, could afford to build even larger footprints along major highways like County Road 58, pulling consumers away from Main Street and the small local retailers that remained. Like many other central business districts, Downtown Riverhead experienced a slow decline in commercial activity.



Historical aerial image of Riverhead showing narrower, consistently-scaled buildings (1947)



Historical aerial image of Riverhead showing department stores and extensive surface parking lots on the waterfront (1984)

Between the 1970s and 1990s, very little development occurred in Downtown Riverhead as a result of this nationwide shift in retail typologies. The 1973 Master Plan recommended a mix of retail, office, and marine-related businesses to create a "Riverhead Business Center" that would be supported with high-density housing around the downtown perimeter. In 1999, resident surveys concluded that Riverhead's focus should shift from shopping to entertainment, tourism, and cultural activities. Construction had begun on Atlantis Marine World (now the Long Island Aquarium), the Suffolk Theater reopened, Hyatt Place opened, and soon after, a Downtown Revitalization Plan was prepared to strategize entertainment clusters. Still, development generally favored other East End towns over Riverhead.

In 2012, Superstorm Sandy hit Long Island, causing long-term power outage and infrastructural damage as well as flooding Downtown Riverhead in three feet of water. The following weeks proved even worse, with water inundating the basement level and often first floor of many of the buildings on Main Street.

The overall pattern of development was largely stifled until 2016, when a new tact was proposed within a Brownfield Opportunity Area Study. Creative placemaking, where special events are strategically planned in order to leverage physical improvements to critical spaces, was recommended for Downtown Riverhead. A robust schedule of themed festivals and other events quickly improved the vibrancy of downtown.



Superstorm Sandy caused a 3-foot flood throughout the low-lying Downtown area



Historical photo of W Main Street, looking east (c. 1910).

Suffolk County Historical Society

Between state and federal grants offering post-Sandy disaster relief and the momentum gained through creative placemaking efforts, four multi-family residential buildings with retail ground floors have been added in Riverhead: Summerwind Square in 2013, Woolworth Apartments in 2015, Peconic Crossing in 2018, and Riverview Lofts in 2020. For the most part, buildings constructed in the last five years are residential or hospitality uses and between four and five stories, a height required for developers to recoup their investment on the cost of construction and land values.

The result of these development patterns is an architecturally diverse streetscape along a heavily-trafficked commercial corridor. Newer multi-family residential buildings match the heights of the older building stock but are generally disliked. Surface parking lots separate activity on Main Street and trails and views on the Peconic River. A wide swathe of former department stores along Main Street remain vacant. Restaurants, theaters, small retail storefronts, and other entertainment uses build off of the excitement from the strong special events programming. Office space remains a high priority due to the county courts. Downtown Riverhead represents a historical commercial corridor with lots of momentum to return to its vibrant past.



History of Development in Downtown Riverhead

CHARACTER OF DEVELOPMENT



Commercial buildings built before 1900 are notable for their tall, transparent storefronts, while institutions like churches are set back from the street.



Commercial and institutional buildings built in the early twentieth century are recognizable for narrower storefronts than earlier buildings.



Mid-twentieth century commercial buildings feature drastically larger footprints and are typically sited near surface parking lots.



The majority of buildings built in the last decade are 4- or 5-story multi-family residential or hospitality uses, with first floor retail or parking.

LISTENING AND OBSERVING

At each stage, we listen, repeat back, and invite input to be incorporated into the Pattern Book.

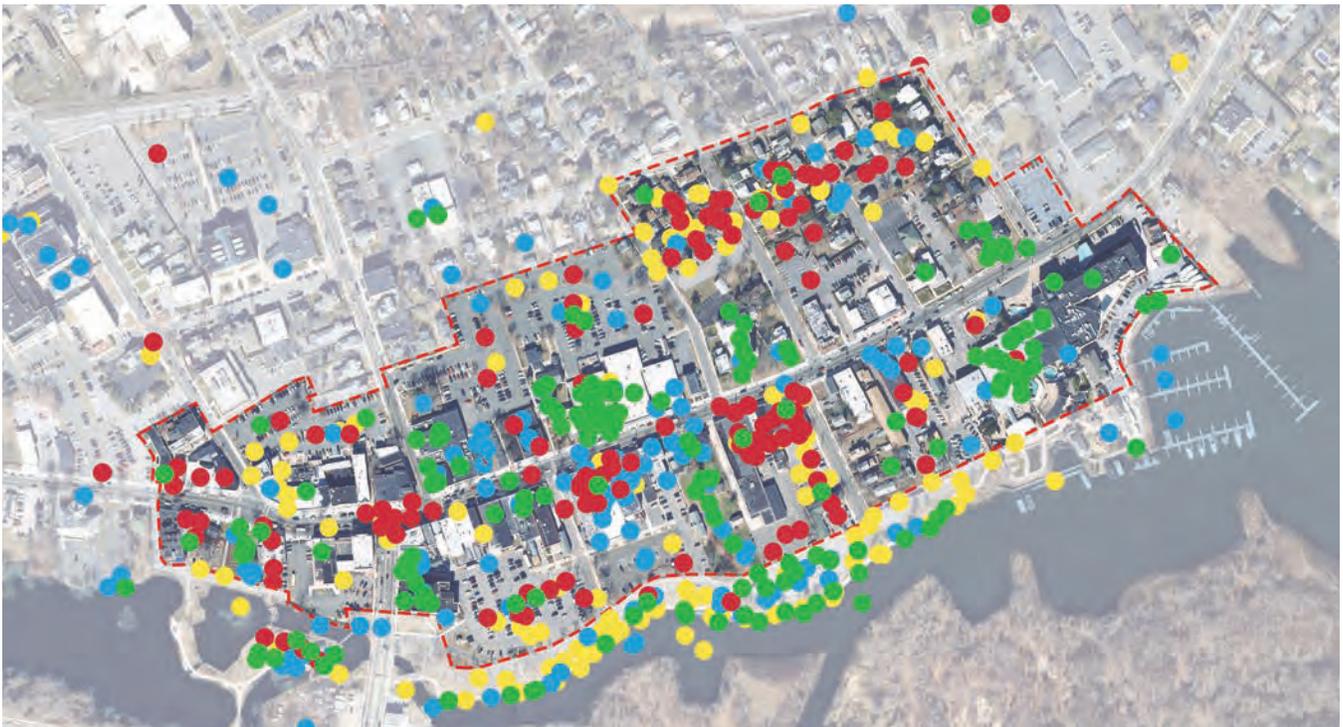
A three-step planning process was applied for the Town of Riverhead. The foundation of this process was authentically listening to stakeholders and residents with multiple feedback loops and inviting all partners to participate in the decision-making process.

Step 1: Listening

- Sharing of data, base-mapping, and recording existing conditions
- Small-group meetings with stakeholders
- One-on-one interviews with Planning Director and elected officials
- Community meeting to solicit resident ideas and visions
- Online Survey to expand outreach beyond community meeting

Step 2: Exploring Ideas

- Incorporation of input from Step 1 into preliminary recommendations



The initial Public Forum sought the opinion of community members using a Dot Exercise to identify strengths, weaknesses, opportunities, and threats.

- Town Council Work Session to compile resources and test ideas
- Follow-up meetings with stakeholders in small groups
- Community meeting #2 to review preliminary recommendations and gather community feedback
- Online Survey #2 to confirm feedback from community meetings

Step 3: Deciding

- Presentation of Pattern Book
- Feedback from stakeholders and residents

Step 4: Implementing

- Town of Riverhead to develop Form-Based Code.

PRECEDENT RESEARCH

In order to understand the regional context of Riverhead, analysis of nearby towns throughout Long Island was conducted early in the community engagement process. Establishing a library of photographs of seven other East End towns provides the opportunity to draw comparisons of public and private features that may be relevant to Riverhead. Contextualization of architectural form and style is also an important aspect of precedent research.

Long Island Precedent Research

- Babylon
- Greenport
- Huntington
- Northport
- Patchogue
- Sag Harbor
- Southampton



Sag Harbor architectural patterns



Recent development in Patchogue

ANALYSIS OF STRENGTHS, WEAKNESSES, OPPORTUNITIES, AND THREATS (S.W.O.T.)

The initial public engagement forum and stakeholder meetings was focused on gathering community feedback regarding Downtown Riverhead's Strengths, Weaknesses, Opportunities, and Threats, commonly called "S.W.O.T. Analysis." Residents and stakeholders marked aerial maps with colored stickers, wrote comments, and shared their visions for downtown. The top five responses to each category are listed below.

Strengths

- Special Events
- Theaters
- Restaurants
- Architectural Character
- Peconic River Waterfront

Weaknesses

- Safety/Perception of Safety
- Traffic
- Current Zoning/Regulations
- Perceived Lack of Parking
- Vacancies

Opportunities

- Riverfront Development
- Town Square or Park
- Small/Local Business Growth
- Increased Multi-Family Uses
- Improved Streetscape

Threats

- Flooding
- Lack of Police Presence/Safety
- Downsizing of Current Zoning
- Parking Supply Not Adequate
- Affordable Housing does Not Support Retail



Strengths



Weaknesses



Opportunities



Threats

ONLINE SURVEYS

To expand the scope of engagement during the Listening phase, two online surveys were developed and advertised via the Town of Riverhead website. The release date and content of each survey directly followed the two initial public community meetings, the first in November 2019 and the second in June 2020.

Survey #1:

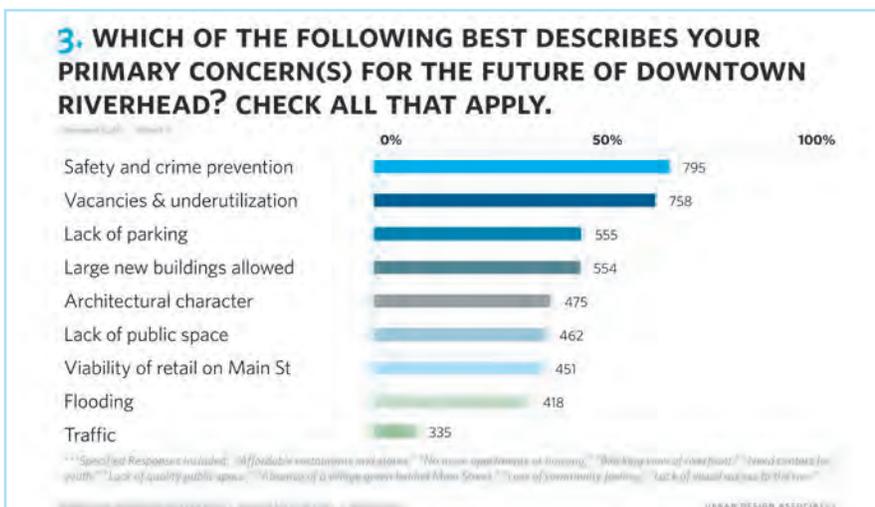
Open January 30th-February 14th, 1,249 total respondents

- Respondent Background, Residence, Primary Concerns
- Strengths, Weaknesses, Opportunities, Threats
- Land Use Preferences
- Major Themes and Concerns

Survey #2:

Open June 1st-June 29th, 409 total respondents

- Respondent Background
- Preferences for Policy, Public Space, and Private Development Improvements
- Architectural Style Preferences



Two online surveys sought input from a wider scope of residents and stakeholders.

This page is intentionally blank.

This page is intentionally blank.



**POLICY
RECOMMENDATIONS**

OVERVIEW

Identifying opportunities to improve existing patterns and establishing plans to mitigate large-scale challenges that may have long-term impacts on Downtown Riverhead starts with policy.

Although non-traditional for a Pattern Book, a focus on policy issues can be a useful mechanism for establishing long-term goals that span across the public and private realms. According to feedback gathered throughout the extensive community engagement process and through research on future development and sea level rise, eight focus areas were determined to be best tackled through policy additions or adjustments. This section delves into a variety of challenges and corresponding recommendations, including:

- Flooding on the Peconic River
- Parking
- Proposed Mix of Uses
- Hypothetical Maximum Build Out
- Vehicular Circulation
- Special Events Programming
- Transfer of Development Rights
- Sustainability and Resiliency



Expansive surface parking lots



New construction parking and flooding considerations



Missing uses in downtown



Vacant Buildings and Lots in Downtown Riverhead (as of publication).

PECONIC RIVER FLOODING

The Peconic River waterfront represents one of Downtown Riverhead’s biggest strengths, offering natural views, recreational amenities, and unique tourism opportunities. Easily accessible from Main Street, the Peconic River is a unique amenity that differentiates Riverhead. But this proximity simultaneously poses a looming threat to downtown buildings. Recurring flooding, ground inundation, and storm surges have impacted many of the buildings on the south side of Main Street in the past and the effects of sea level rise project far worse situations in the future.

The International Panel on Climate Change (IPCC), a UN body tasked with assessing global climate risk, estimates that global sea levels will rise about one foot by the year 2050 at the current rate of industrial pollution. By the year 2100, projections double. Additionally, the severity of storms and other major weather patterns is expected to increase. At a global scale, perhaps these projections aren’t too daunting; however, at a local scale, this will wreak havoc on already-burdened building infrastructure and communities. In Riverhead, sea level rise of just 1 foot in 30 years would reach approximately seven buildings on the south side of Main Street. A rise of two feet would impact about twenty buildings. It’s difficult to project the impact of a severe storm on top of these sea level rise projections, but the effects of any major storm event could be disastrous for Downtown Riverhead.

RECOMMENDATIONS

In April 2020, town representatives had an initial meeting with the U.S. Army Corps of Engineers to discuss the existing flood risk and an approach to developing an action plan. There are five types of options for adapting to sea level rise:

- Planning;
- Regulatory;
- Land use change or restrictions;
- Structural; and
- Environmental.

Given the proximity of downtown to the Peconic River and the elevation of Main Street relative to high tide, Riverhead’s options are limited. The Town should seek funding for developing and implementing a sea level rise resiliency plan with guidance from the Army Corps. In the meantime, the Town should establish objective standards for new buildings that regulate how far conditioned space and mechanical equipment should be located above the current floodplains.



Town of Riverhead Chamber of Commerce

Heidi Behr Way following Superstorm Sandy



Riverhead LOCAL

Grangabel Park following Superstorm Sandy



Riverhead News Review

Peconic Avenue following Superstorm Sandy



Riverhead News Review

Hyatt Place pool following Superstorm Sandy



2020 Current shoreline.



2050 Estimated shoreline 2050 (about 1 foot above current sea level).



2100 Estimated shoreline in 2100 (about 2 feet above current sea level).

PARKING

According to community feedback, many residents and stakeholders agree that parking resources around downtown need to be better balanced, with larger parking lots on the south side of Main Street rarely filled to capacity and lots on the north side regularly filled. Residents also expressed a need for more handicapped parking spaces and better signage denoting parking lots. Many residents positively responded to the idea of a parking garage on the north side of Main Street, specifically if more residential units are to be built downtown. Although current peak demand only burdens some lots, most residents agreed that there is a need for more parking spots near key downtown destinations, including the courts and major restaurant and retail clusters, such as the lot behind Digger's.

RECOMMENDATIONS

According to the Strategic Parking Plan prepared for the Town of Riverhead in 2019, current peak demand does not exceed 58% of capacity, meaning that constructing additional parking at this time is not necessary. Try these measures, per the Strategic Parking Plan, before building more parking:

- Prioritize spaces for customers and visitors in centrally located facilities.
- Update enforcement efforts to better monitor and manage parking demand.
- Establish regulations for incoming developments to prepare for increased residential parking demand as shared parking does not work well for residential units. This would include requiring that all new residential construction downtown provide adequate on-site parking for residents. Alternatively, developers should have the option to pay into the parking district for their pro rata share of downtown parking.
- Enhance alternative transportation amenities to reduce parking demand.

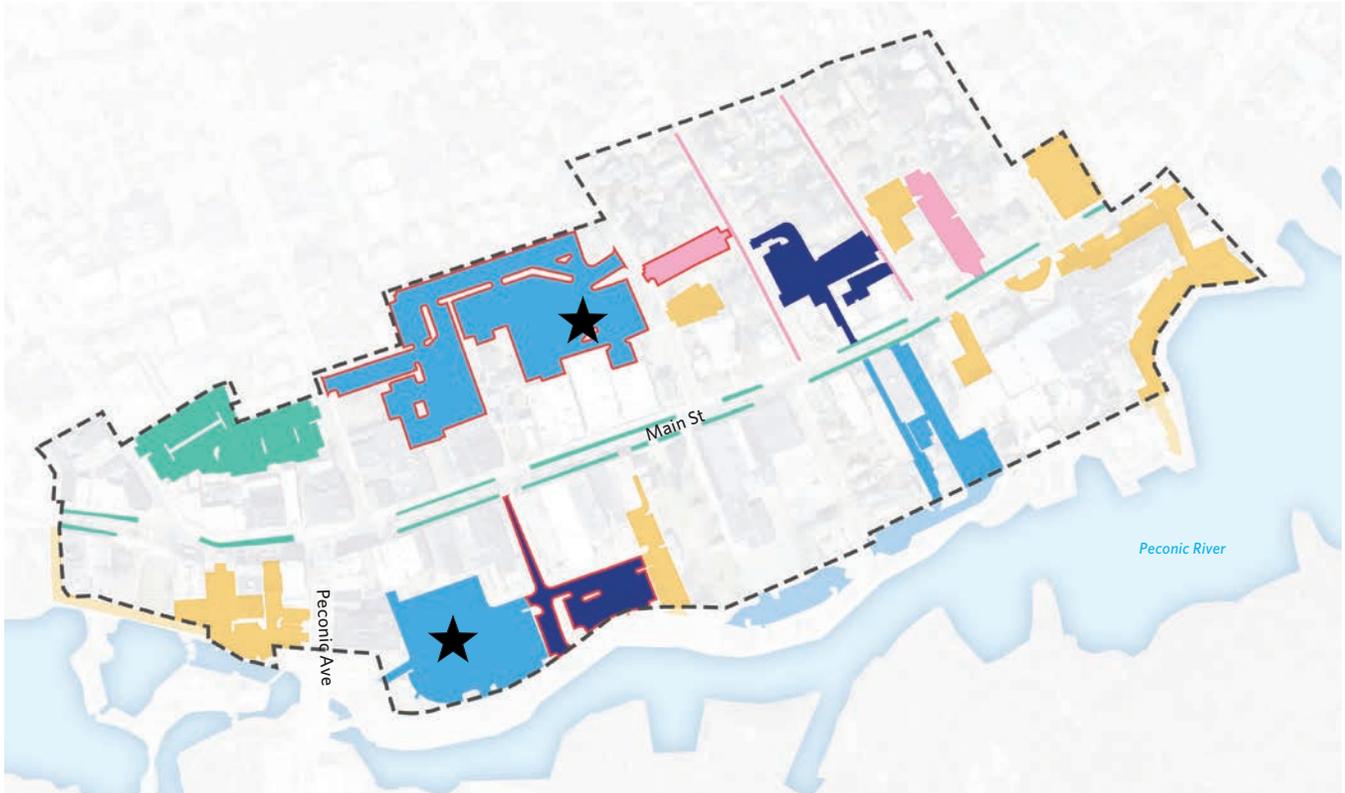
Note: Parking garages north and south of Main Street should be considered when demand warrants.



Expansive surface parking lots



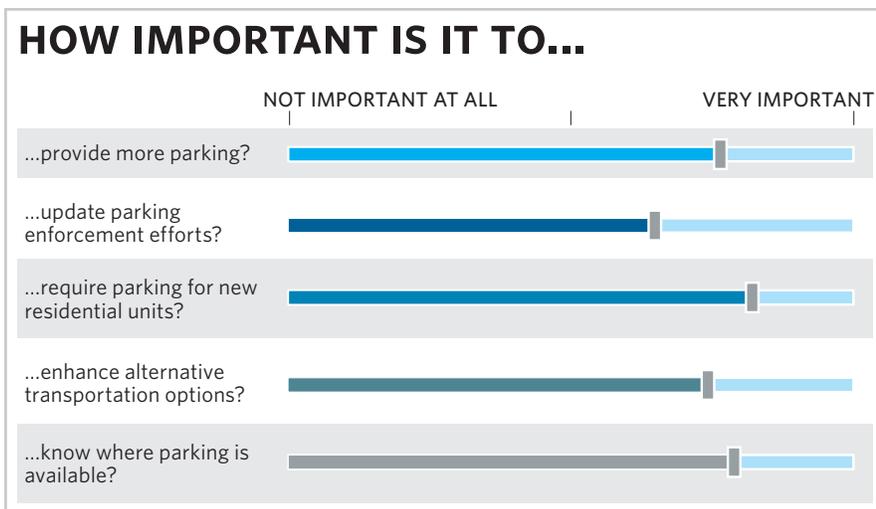
Typically crowded parking lot near courthouse



Recommended Parking Regulations, as per the Strategic Parking Plan prepared for the Town of Riverhead (2019).

- ★ POTENTIAL FUTURE PARKING GARAGE LOCATION
- 4-HOUR PARKING
- 3-HOUR PARKING
- 2 HOUR PARKING
- FREE/UNRESTRICTED PARKING
- PRIVATE PARKING LOTS
- PROPOSED PERMIT PROGRAM FACILITIES

POLICY RECOMMENDATIONS



Results from question #3 of the second online survey.

PROPOSED MIX OF USES

Like other historical commercial corridors in the country, Downtown Riverhead has experienced many iterations of retail landscapes. But due to a shift in focus from pedestrian to vehicular travel in the last few decades, larger building footprints, more surface parking area, and national branding strategies have become favored by larger (big box) retailers. All these aspects are more readily available off of County Road 58 than on Main Street. At the same time, Main Street evolved into the home of unique experiences.

Additionally, the COVID-19 pandemic has drastically impacted the development landscape, potentially permanently. Restaurants, bars, retail, hotels, and office space—all staples of Downtown Riverhead—have been disproportionately impacted. The future is uncertain; however, it's clear that the COVID-19 pandemic has amplified trends, such as the demise of traditional malls. Meanwhile, the move to telecommuting by most office workers has already made lives more home-centric. For many, the hurdle of commuting may be permanently removed, which should make living and working from home in Downtown Riverhead a more attractive option for many. Although somewhat at odds with the results from the community surveys, residential uses within downtown may be the most risk-averse choice from a development perspective.

Regardless, unique predictable positive experiences, such as the unique mix of cultural uses and civic amenities with small-scale retail with food and beverage, remain the answer for the long-term future. Smart insertions of cultural amenities plant the seed for a more vibrant downtown to serve residents of downtown, the Town, and regional visitors.

RECOMMENDATIONS

Focus on supporting and adding unique uses that C.R.58 and other nearby downtowns don't have:

- Existing cultural destinations (LI Science Center, Suffolk Theater, SCCC Culinary Arts, East End Arts, Vail-Leavitt Music Hall)
- New cultural and recreational destinations (museum collections, evening entertainment, arts & music, parks, trails, etc.)
- Additional unique local food and beverage
- Ancillary local retail
- Additional hospitality
- Postsecondary education
- Upper story office
- Wide range of downtown living opportunities and choices



East End Arts



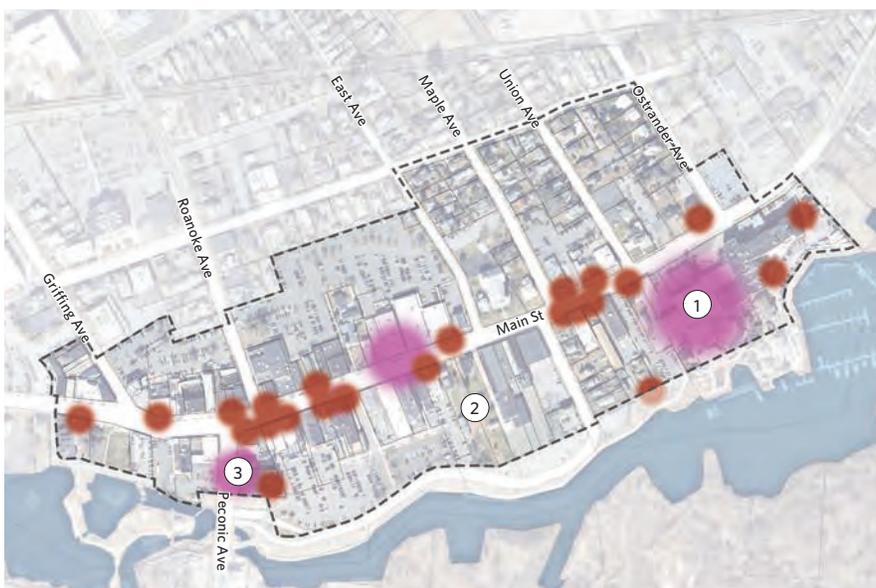
Starr Confectionery



The Suffolk Theater



Downtown is characterized by a broad mix of uses.



- OFFICE
- MULTI-FAMILY
- RETAIL
- INSTITUTIONAL
- HOSPITALITY
- CONVERTED HOUSES

POINTS OF INTEREST

- ① AQUARIUM
- ② EAST END ARTS
- ③ DARK HORSE

Existing Heat Map of Restaurants (red) and Entertainment (purple).

MAXIMUM BUILD OUT

Current DC-1 zoning permits 5-story buildings with up to 80% lot coverage (applicants may apply for 100% lot coverage). Community surveys revealed that many residents prefer buildings to appear smaller in scale, in keeping with the historical fabric. Balancing these aesthetic preferences with financial limitations of smaller-scale redevelopment has generated much discussion about the future of Downtown Riverhead. A hypothetical analysis of the potential build out for the overlay district revealed that Downtown has the potential to significantly increase density under current zoning. If built out, there are three likely types of redevelopment scenarios:

- Redevelopment of vacant lots and buildings;
- Redevelopment of parking district parking lots; and
- Redevelopment of key opportunity sites (existing 1- and 2-story buildings with no historic architectural value).

FINDINGS

- Existing buildings could be dwarfed by new buildings unless objective standards are developed and the existing zoning is changed.
- Development on parking district lots may require surface parking to be replaced in parking garages to accommodate replacement parking and any new development requirements.
- Five-story buildings along the length of Main Street could create a canyon-like effect and cast shadows on streets, sidewalks and outdoor dining, creating an undesirable condition for pedestrians.
- Net new development could effectively double built square footage in Downtown Riverhead and accommodate approximately 1,200 rental apartments (assuming a 50/50 mix of 1- and 2-bedrooms).

EXISTING CONDITIONS



Existing Aerial: Primarily 1-3 story buildings front Main Street.



Typical Existing fabric of downtown Riverhead



Vacant buildings provide an opportunity for redevelopment



5-story 80% lot coverage buildings are permitted under current zoning



- STUDY AREA
- EXISTING BUILDING
- EXISTING VACANT BUILDING
- REDEVELOPMENT OF VACANCIES
- REDEVELOPMENT OF PUBLIC PARKING
- REDEVELOPMENT OF OPPORTUNITY SITES

Square footages are approximate

HYPOTHETICAL MAX BUILD OUT

Redevelopment of Vacant Lots and Buildings

	New Development	Demolition	Net New
Ground Floor (SF)	82,000	36,000	46,000
Upper Floor (SF)	260,000	13,000	247,000
Total (SF)	342,000	49,000	293,000
Single Family Homes (#)	0	0	0

Redevelopment of Public Parking Districts

	New Development	Demolition	Net New
Ground Floor (SF)	85,000	0	85,000
Upper Floor (SF)	313,000	0	313,000
Total (SF)	398,000	0	398,000
Single Family Homes (#)	0	0	0

Redevelopment of Opportunity Sites

	New Development	Demolition	Net New
Ground Floor (SF)	180,500	197,100	-16,600
Upper Floor (SF)	672,000	72,000	600,000
Total (SF)	852,500	269,100	583,400
Single Family Homes (#)	0	6	-6

Total

	New Development	Demolition	Net New
Ground Floor (SF)	347,500	233,100	114,400
Upper Floor (SF)	1,245,000	85,000	1,160,000
Total (SF)	1,592,500	318,100	1,274,400
Single Family Homes (#)	0	6	-6



Hypothetical Maximum Build-out Plan.



Hypothetical Maximum Build-out Aerial: 5-story buildings infill vacancies, public parking districts, and existing underutilized buildings.

TRANSPORTATION

Main Street, a section of NY-25, is a heavily-trafficked arterial that runs east to west along almost the full length of Long Island. Notably, one of the last crossing points between the North and South Forks is located where Main Street meets Roanoke Avenue and Peconic Avenue, right at the historical center of Downtown Riverhead. The older building stock that forms this crucial intersection restricts the width of the rights-of-way, therefore limiting the number of travel lanes. Despite the plentiful access and heavy use, community feedback reported that Main Street is not always perceived as the pedestrian-friendly, small-scale commercial corridor that it used to be due to the speed of cars and lack of pedestrian crossings. With only four stoplights on Main Street within the DC-1 Zoning Use District, drivers often exceed the speed limit of 30 miles per hour and can easily miss the businesses, institutions, and other activities in downtown.

Public transportation options in Riverhead are limited to the LIRR which is about a half-mile northeast of the DC-1 district (an easy walking distance from downtown) and a single bus line on Main Street. Public transit within the downtown area was noted as a weakness by the residents and by the Parking Study.

RECOMMENDATIONS

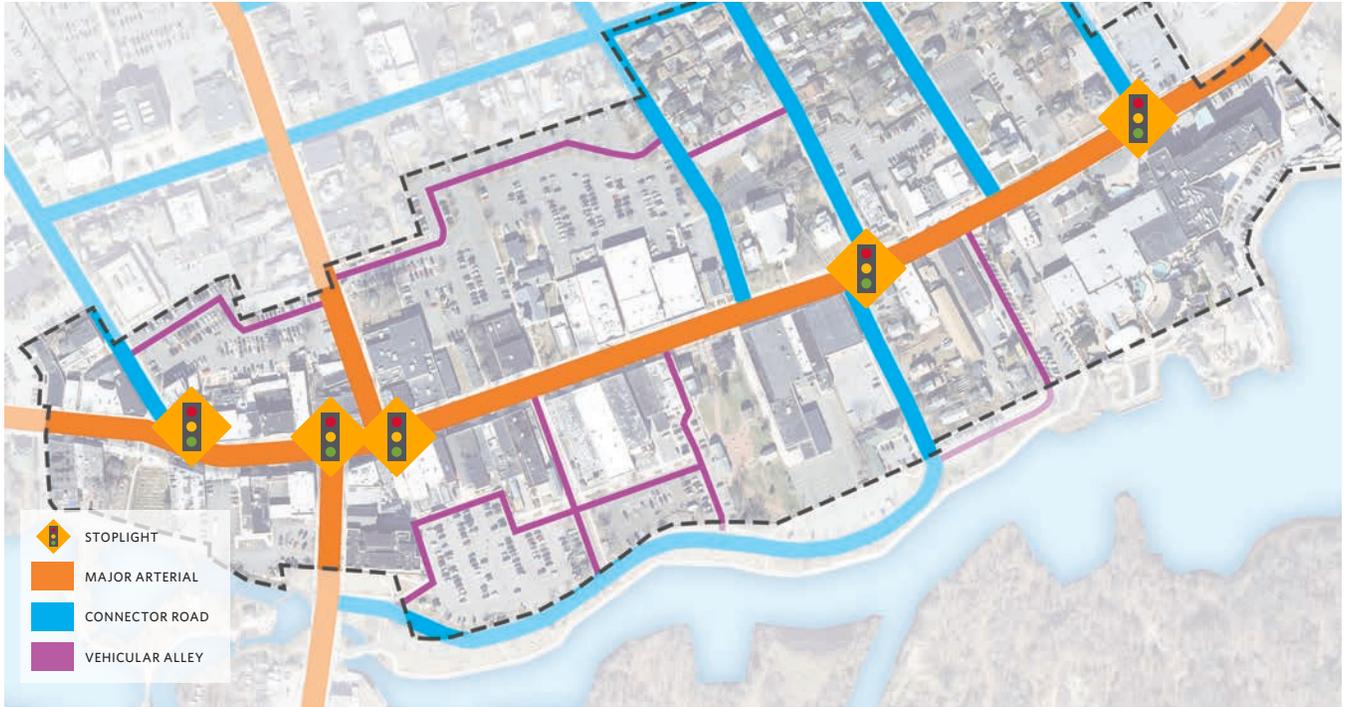
Major intersection improvements would require the demolition of historical, occupied buildings: something that is politically and economically improbable. The existing street layout is therefore likely to remain. However, the town should work with the State and other agencies on ways to calm the speed of traffic on Main Street.

Alternative transportation options should be studied as well:

- More frequent LIRR service should be considered.
- More frequent bus service should be considered.
- A shuttle connecting remote parking facilities, the LIRR station, and Main Street should be considered, as per the 2019 Parking Study.
- Areas intended for rideshare/carshare services to pick up and drop off along Main Street should be considered.
- More clear and consistent bicycle circulation should be implemented.
- More clear and consistent pedestrian circulation should be implemented.
- Wheelchair and other mobility limitations should be accommodated in all public spaces, sidewalks, and pedestrian street crossings.



LIRR Frequency of Use



Vehicular Circulation



Public Bus Route & Proposed Shuttle Route

SPECIAL EVENTS

According to public community meetings and online survey outreach, most residents agree that special events are one of Downtown Riverhead's greatest strengths and attract visitors otherwise unlikely to visit. The existing robust events schedule consists of seasonal and themed festivals like Alive on 25 that are mostly concentrated in the summer months. For many of these special events, the length of Main Street from Griffing Avenue to Ostrander Avenue is closed to vehicular traffic, necessitating temporary special traffic patterns. Some owners of restaurants and small businesses along Main Street report a lack of access to parking for regular customers, while many residents appreciate the value the pedestrian-only Main Street corridor for events. The overall consensus recognized the value of special events to bring a community together as well as drawing visitors from around the region to downtown who might not otherwise travel to Riverhead.

RECOMMENDATIONS

- Events programming should remain robust and consistent; especially as the local and state governments permit outdoor dining and congregation post-COVID-19 pandemic, focus should remain on community gathering
- Detours and parking areas should be better marked for special events
- Event coordinators should consult with local restaurant and business owners to identify overlaps in festival themes how business owners can be more engaged.



Alive on 25



Street Painting Festival



Reflections: Art in the Park



Polish Festival

SHOULD RIVERHEAD HOLD MORE, LESS, OR THE SAME NUMBER OF SPECIAL EVENTS?

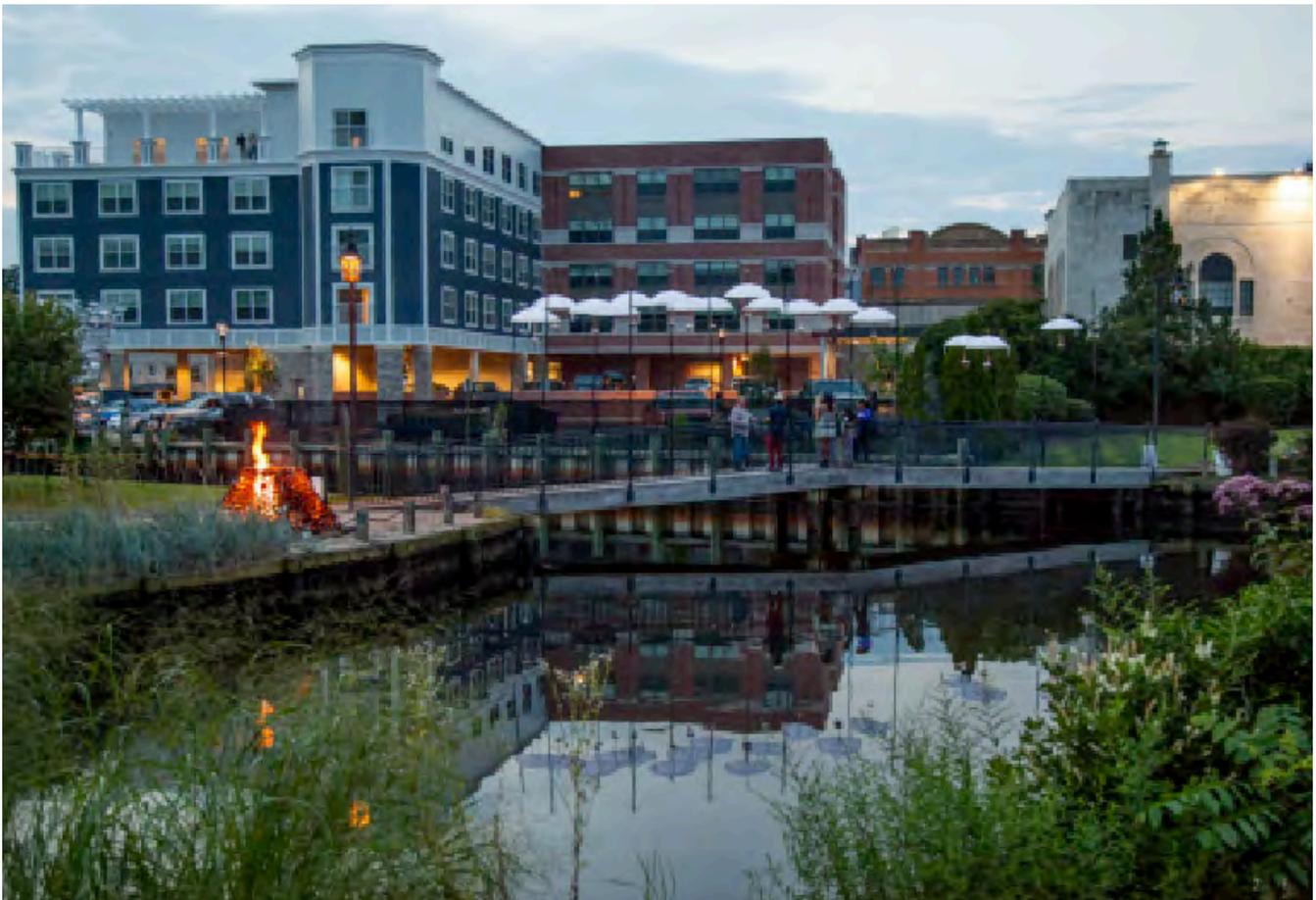
Answered: 780

Skipped: 469



The average percentage of respondents' ranking was 78%

The first survey invited respondents to give their opinion on the number of special events that are currently held in Downtown Riverhead, particularly focused around Main Street; the existing programming is consistent and robust, providing a year-long series of events aimed at a diverse audience of locals and seasonal tourists, adults and children, and more.



Town of Riverhead Chamber of Commerce

Reflections: Art in the Park event photo.

TRANSFER OF DEVELOPMENT RIGHTS

Transfer of Development Rights, or TDR, is a zoning approach used to protect land with conservation value (in this case, farmland) by redirecting development that would otherwise occur on this land to an area planned to accommodate growth and development (such as the downtown DC-1 zone).

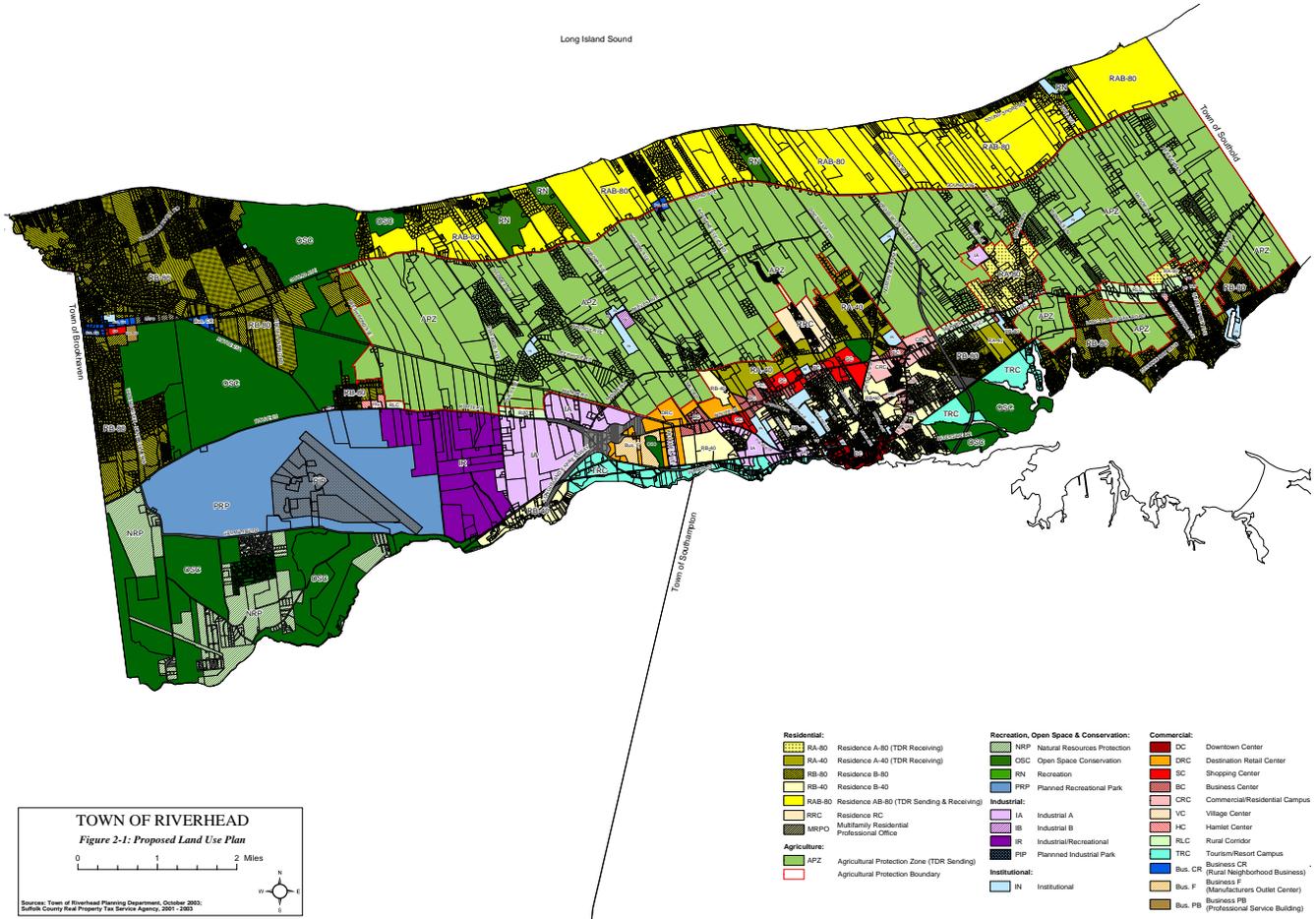
TDR programs financially compensate landowners, such as farmers, for choosing not to develop some or all of their land. The land is permanently protected through a conservation easement or a restrictive covenant. The development value of the land where the transferred development rights are applied is enhanced by allowing for new or special uses, greater density or intensity, flexibility with zoning areas and bulk standards (variations on setbacks, lot sizes, etc.), increased floor-area allowances, increased building-height allowances, and maximum impervious lot coverage allowances.

The concept behind TDR is that everyone benefits:

- The farmer receives financial compensation for conserving their land
- The developer makes a profit by taking advantage of increased flexibility
- The community enjoys the many benefits of conserved land
- Traditional sources of conservation funding can be used to protect additional important properties.

FINDINGS

- TDR can be an effective tool for land conservation.
- A successful TDR policy requires a consistent long-term approach on the part of the town.
- In Riverhead's DC-1 Zoning Use District, most potential TDR benefits are already included as of right in the zoning ordinance, leaving limited incentive for developers.
- Local developers questioned whether downtown infill would support the increased burden of TDR.



Land Use as per 2003 Town of Riverhead Comprehensive Plan.

Proposed Land Use Plan, Town of Riverhead Comprehensive Plan 2003

POLICY RECOMMENDATIONS

SUSTAINABILITY

In July 2019, the Town Board of Riverhead adopted a resolution taking the “climate-smart pledge,” the first step in New York State’s “Climate Smart Communities” certification program. This pledge is designed to support local governments in communities impacted by climate change and addresses ten major areas (see recommendations below). While these general goals are helpful in positioning the Town’s policies and grant funding, they do not provide concrete actionable goals. With issues of stormwater runoff, ground inundation, storm surges, and recurring flooding, Downtown Riverhead faces imminent challenges that may be mitigated on a smaller scale.

RECOMMENDATIONS

- The goals outlined in New York State’s Climate Smart Communities certification program should be used as a scorecard for smaller-scale projects to focus on the ten areas outlined in the certification framework:
 - Build a climate-smart community
 - Inventory emissions, set goals, and plan for climate action
 - Decrease energy use
 - Shift to clean, renewable energy
 - Use climate-smart materials management
 - Implement climate-smart land use
 - Enhance community resilience to climate change
 - Support a green innovation economy
 - Inform and inspire the public
 - Engage in an evolving process of climate action
- Encourage new buildings to meet the 2030 Challenge.
- In existing buildings that cannot be raised to avoid flooding, flood-proofing strategies should be explored.
- Invest in smaller-scale interventions to help mitigate stormwater and floodwater issues. These interventions may include:
 - Rework surface parking lots, flat roofs, and other large impervious surfaces to either detain stormwater or allow water to seep into the ground using bioswales, park space, and/or green roofs.
 - Require new buildings in the DC-1 district to accommodate floodwater on the ground floor by requiring high-efficiency mechanical units on roofs instead of in basements.



Recent residential development on Main Street designed to accommodate some flooding on the ground floor

2030 CHALLENGE TARGETS

- All new buildings, developments, and major renovations shall be designed to meet a fossil fuel, GHG-emitting, energy consumption performance standard of 70% of the regional (or county) average/median for that building type.
- At a minimum, an equal amount of existing building area shall be renovated annually to meet a fossil-fuel, GHG-emitting, energy consumption performance standard of 70% of the regional (or county) average/median for that building type.
- The fossil fuel reduction standard for all new buildings and major renovations shall be increased to:
 - 80% in 2020
 - 90% in 2025
 - Carbon-neutral in 2030 (using no fossil fuel GHG-emitting energy to operate).



Impervious Surfaces include impermeable paving like asphalt as well as building roofs and prevent rainwater from naturally seeping into the ground.

POLICY RECOMMENDATIONS



Example of stormwater detention within a pedestrian/parking area.



Example of a retail building designed to accommodate some flooding on the ground floor using garage door-style openings at the lowest elevation, tile and concrete floors and walls, and elevating equipment above the floodplain.

EvolveEA's Millvale Food Hub (2016)

This page is intentionally blank.



GUIDELINES FOR PUBLIC AREAS



OVERVIEW

A vibrant public realm is key to the success of any main street. Active storefronts, safe and beautiful open spaces, and cohesive circulation patterns all play a part in creating a successful urban environment.

A town's main street is its center for activity. And while restaurants, boutiques, coffee houses, offices, and residences are all important components to a viable downtown, it is the public spaces—the parks, crosswalks, streets, sidewalks, and paths that foster a successful urban environment. Curating a uniquely Riverhead experience is essential to stand out from other regional downtowns. Public areas should be designed with the pedestrian in mind and should encourage retail patronage and safe gathering.

Specific attention should be paid to the following areas:

- **Vehicular Circulation:** Vehicular traffic should support downtown Riverhead versus detracting from the experience. Vehicles should move smoothly and slowly along Main Street.
- **Pedestrian Circulation:** Pedestrians should move with ease throughout Riverhead. Sidewalks and crosswalks should provide a cohesive and safe environment.
- **Bicycle Circulation:** Downtown Riverhead should encourage biking as a means of mobility, and improve connections with the larger regional bicycle network.
- **Pedestrian-Friendly Streets:** Sidewalks, streets, and storefronts should provide a vibrant and inviting experience in downtown Riverhead.
- **Pedestrian-Friendly Passages:** Passages should be amenitized to improve safety and encourage visitors to use mid-block connections.
- **Parks and Open Space:** Existing parks and open spaces should be connected and amenitized to improve the user experience.
- **Public Safety:** Measures should be taken to improve the perception of safety in the downtown area.
- **Town Square:** A new town square would provide a central space for gathering as well as a connection between Main Street and the Peconic River.



Regional examples of animated, welcoming, lively pedestrian-focused areas



Downtown Riverhead's Grangabel Park features a plaza with bench seating and pleasant walkways that meander through arts installations.



Outdoor restaurant seating in wider passages creates an opportunity to animate underused spaces.



River and Roots Community Garden is decorated and maintained to appear welcoming.

VEHICULAR CIRCULATION

Successful downtowns rely on multiple means of mobility (train, bus, ride-share, bicycle, etc.). Located at an important regional nexus and primarily centered on one street, it's the circulation of cars with plentiful visibility and access to local businesses, institutions, and other amenities that is paramount to the success of Downtown Riverhead. But according to community feedback, there's room for improvement. Traffic was one of the top weaknesses of downtown listed by residents; due to its location between the North and South Forks, thousands of vehicles pass through the intersection at Roanoke Avenue, Main Street, and Peconic Avenue daily but treat the area as a thoroughfare rather than a destination. As a result, vehicular speeds regularly exceed posted speed limits, hurting the economic success of destinations and comfort of pedestrians on Main Street. Additionally, historical unaligned intersections along Main Street cause confusion and congestion.

GUIDELINES

- Encourage slower-moving traffic through Main Street in order to draw more visitors to the businesses, cultural attractions, and other downtown destinations along main thoroughfares. Cars traveling at slower speeds also benefits the users of other means of mobility, primarily cyclists and pedestrians. Vehicular speed-reducing measures can include:
 - Add special pavement, raised, and/or lighted crosswalks at key intersections to denote pedestrian crossing areas.
 - Increase on-street parking to create a visual barrier between vehicular and pedestrian realms.
 - Replace striped "no parking" areas in the parking lanes on Main Street with curb bump-outs.
 - Add other curb bump-outs to visually and physically narrow the travel lanes along Main Street.
- Work with the State to study existing traffic patterns and congestion at the intersection at Peconic Avenue, Main Street, and Roanoke Avenue. Consult the recommendations made in the Traffic Impact Study TOD Growth Plan for Riverhead prepared in 2015 and the Riverhead Parking Plan Preliminary Recommendations prepared in 2019.
- Provide additional wayfinding signage to help vehicles navigate, specifically indicating parking locations, major local destinations, and alternative routes for drivers intending to bypass downtown.



U.S. Department of Transportation

Example of signalized crosswalk



GreenWorks

Example of curb bump-out



WALC Institute

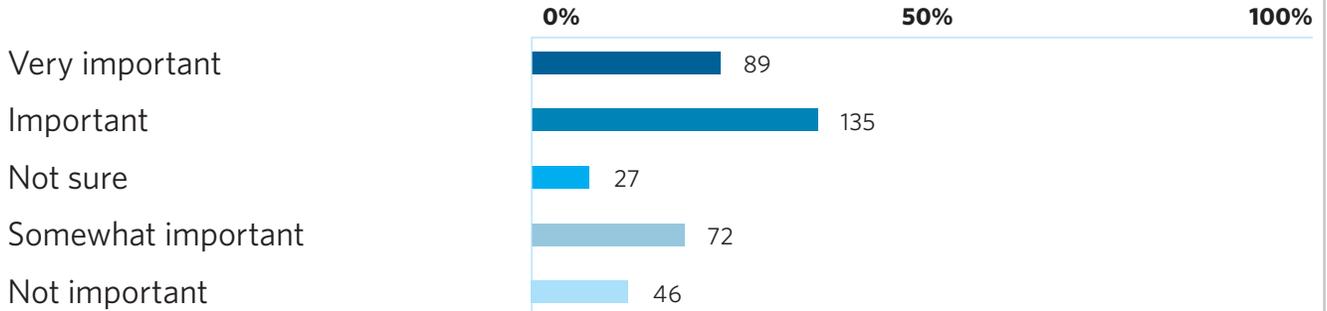
Example of wayfinding signage to direct traffic



Example of painted pedestrian crossing in Patchogue to slow drivers at key crossings between the library and movie theater

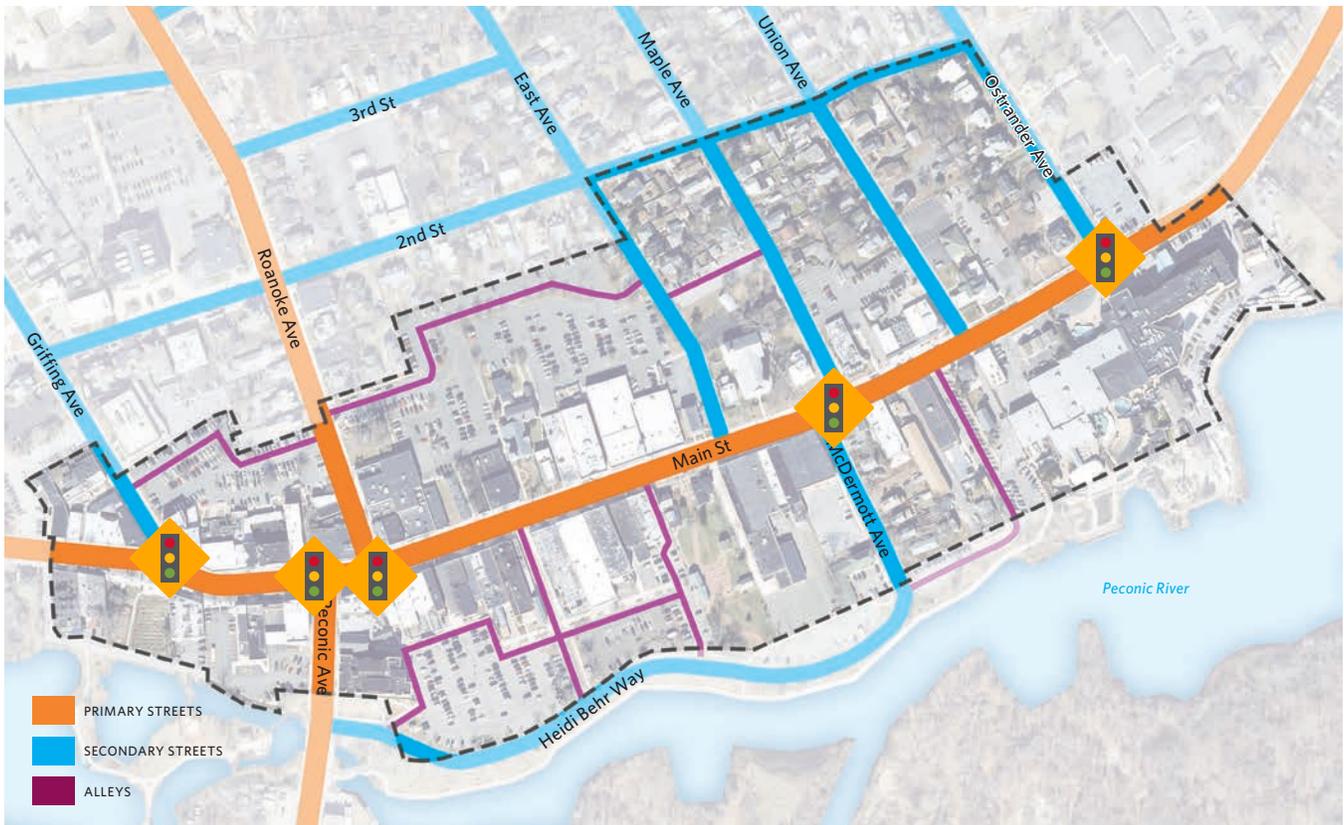
HOW IMPORTANT IS IT TO CALM THE SPEED OF TRAFFIC ON E MAIN STREET?

Answered: 369 Skipped: 40



About 60% of respondents rated this issue as “Important” or “Very important”

The seventh question of Survey #2 asked residents about the importance of the issue of speeding cars along Main Street.



Downtown Riverhead’s street network features three major layers of streets.

PEDESTRIAN CIRCULATION

The pedestrian experience is key to a successful Main Street. Visitors should feel safe to stroll between restaurants, offices, parks, and attractions. According to community feedback, additional crosswalks are needed. Existing crosswalks at non-signalized intersections are challenging for pedestrians due to free-flowing traffic.

RECOMMENDATIONS

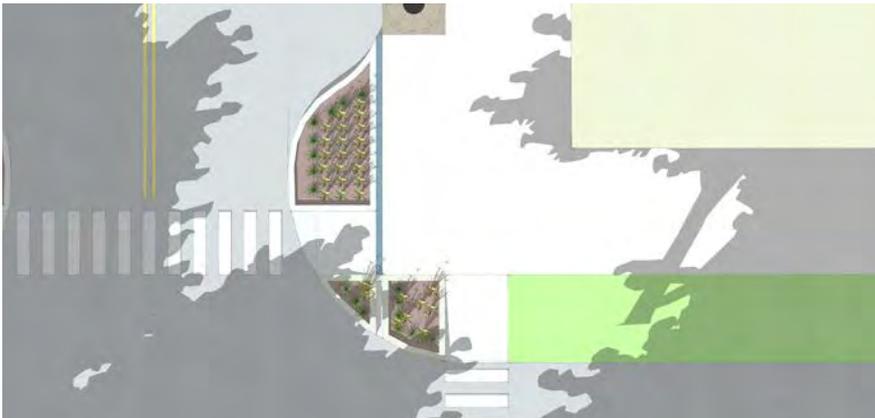
- Ideally, crosswalks should be a maximum of 400 feet apart. Currently, the stretch of Main Street between Union and Ostrander Avenues exceeds this rule. A crosswalk should be considered at Union Avenue.
- Work with the State to add pedestrian actuated flashing lights at all non-signalized (mid-block) crossings.
- Add curb bump-outs into the parking lane at mid-block crossings to increase pedestrian visibility and safety.
- Consider replacing striped "no parking" areas in the parking lane on Main Street with curb bump-outs/landscaping.
- Consider adding temporary and/or permanent parking space parks.



Pedestrian crossing across Peconic Avenue that could benefit from the addition of pedestrian-activated flashing lights



Example of a curb bump-out



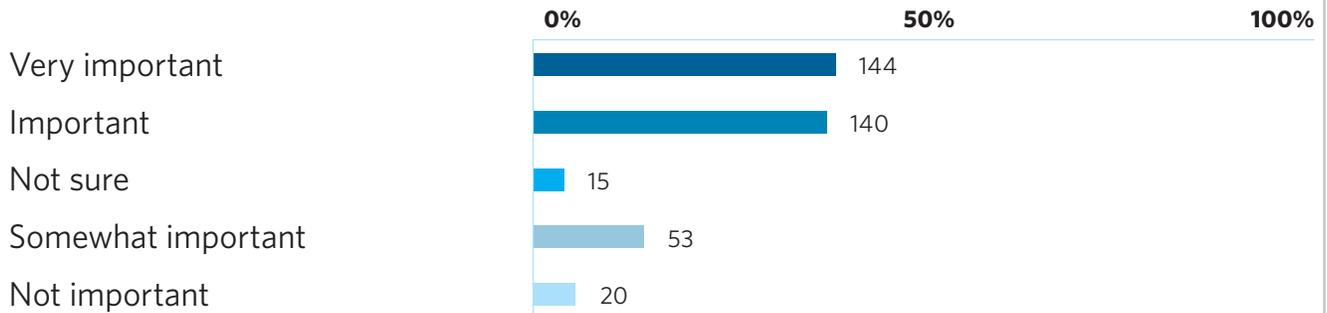
Plan view of a curb bump-out.

CURB BUMP-OUT ELEMENTS

- Shortens distance for pedestrians to cross the street
- Provides an opportunity for beautification through landscape
- Formalizes existing on street parking and creates increased protection for parked cars

HOW IMPORTANT IS IT TO IMPLEMENT TRAFFIC CALMING MEASURES TO IMPROVE THE SAFETY OF E MAIN STREET'S PEDESTRIAN CROSSINGS?

Answered: 372 Skipped: 37



Over 75% of respondents rated this issue as "Important" or "Very important"

The eighth question of Survey #2 asked residents about the importance of the issue of pedestrian crossing safety along Main Street.



This proposed pedestrian activity plan may provide improved pedestrian experiences: a key to a healthy downtown.

BICYCLE CIRCULATION

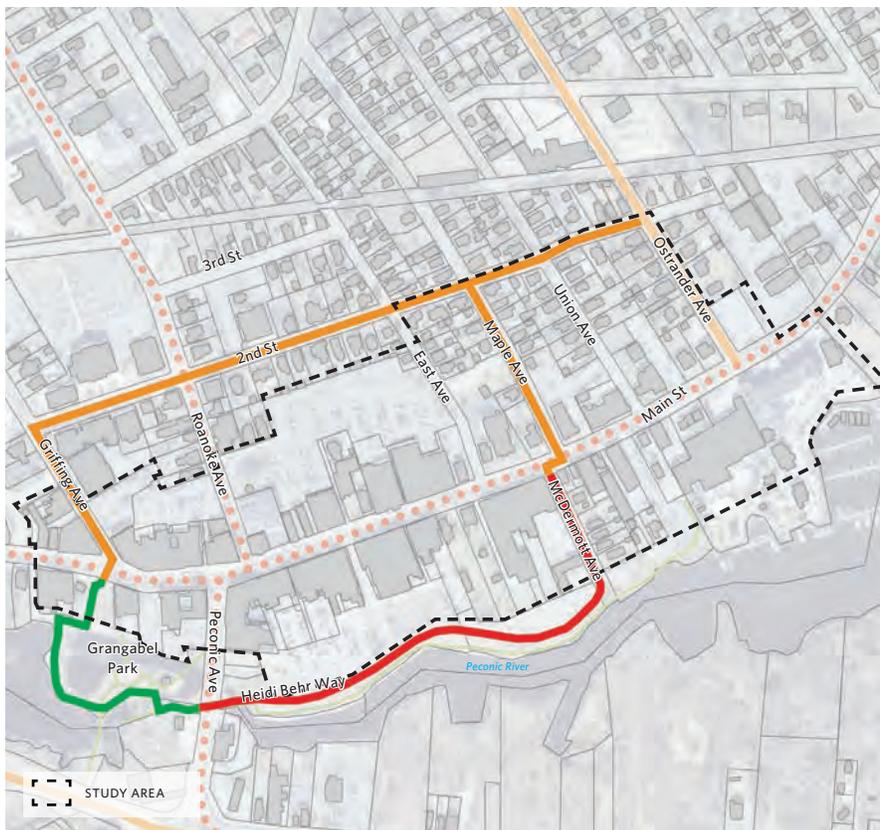
Bicycling is an environmentally-responsible means of mobility, as well as a healthy and fun way to explore Riverhead. To lessen vehicle traffic, decrease Riverhead’s carbon footprint, and encourage healthy living, cycling should be an encouraged transportation method.

As a region, Long Island boasts a robust series of bicycle paths, lanes, and routes which connect neighboring towns. Attractions such as parks, universities, campgrounds, beaches, and shopping are available along these bikable routes. The region also encourages cycling by providing marked routes to park and ride facilities, as well as bicycle lockers at key locations. Like any transportation network, Downtown Riverhead’s local bicycle circulation network must be considered as a piece of the larger regional network; currently, the local and regional networks are relatively disconnected.

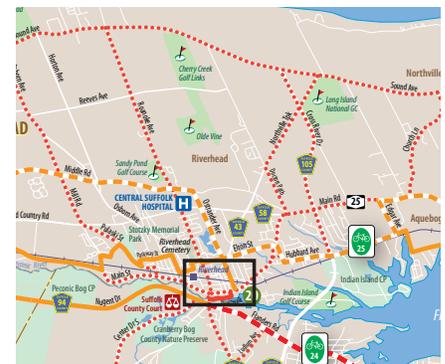
According to community feedback, biking in Downtown Riverhead is inconvenient and sometimes dangerous. The lack of bike lanes or marked routes plus free-flowing car traffic deters cyclists from downtown.



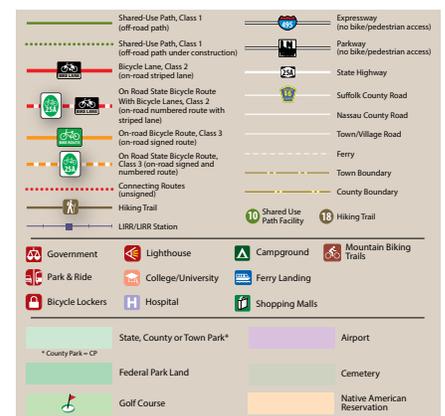
Example of wayfinding signage directs cyclists to bike-friendly streets and trails



Proposed bike connections in Downtown Riverhead create a bikable loop around Main Street which provides a safer environment for cyclists in the downtown area. Existing bike connections are shown faded out beneath the proposed loop.



Existing regional bike plan



Legend

RECOMMENDATIONS

Several improvements should be implemented to encourage a safe and welcoming environment for cyclists, including:

- Create a bike loop around the most heavily-trafficked downtown streets (Main Street, Peconic Avenue, and Roanoke Avenue). This should include:
 - **Class III Bike Routes** on Maple, E 2nd, and Griffing Avenues
 - **Class II Bike Lane** on McDermott Avenue
 - **Class I Bike Path** through Grangabel Park
- Provide clear signage to guide cyclists to bicycle-friendly routes.
- Implement traffic-slowing techniques to create safer conditions for cyclists.
- Install bike racks along Main Street, along the riverfront, in parks, and in the new town square.



Examples of appropriate bike infrastructure



Main Street is designated an unsigned connecting route.



Roanoke Avenue is designated an unsigned connecting route.



Ostrander Avenue is designated a Class 3 Bicycle Lane (on-road signed route).



Heidi Behr Way/McDermott Avenue is designated a Class 2 Bicycle Lane (on-road striped lane).

PEDESTRIAN-FRIENDLY STREETS

While Downtown Riverhead is home to great shops, restaurants, and entertainment, community feedback reported that the current streetscape is somewhat dull and lacking in character. With limited street furniture, outdoor seating for restaurants, shade, plantings, or inviting storefront displays, passersby are not encouraged to linger and therefore are less likely to patronize businesses. Lifeless streets and sidewalks have a snowball effect: if fewer people are present outside of businesses, vibrancy decreases, and people are more likely to choose to shop and dine elsewhere.

RECOMMENDATIONS

Main Street is a regional destination for dining, shopping, and entertainment. Lively and beautiful sidewalks are crucial to creating and sustaining a meaningful experience. In general, sidewalks can be broken into three parts: the Amenity Zone, the Clear Zone, and the Marketing Zone. The Clear Zone is reserved primarily for pedestrian traffic and should be left unimpeded. The Amenity and Marketing zones, however, provide a great opportunity for beautification and better utilization of outdoor spaces for dining, communing, and socializing.

Potential Investments to Improve The Amenity Zone

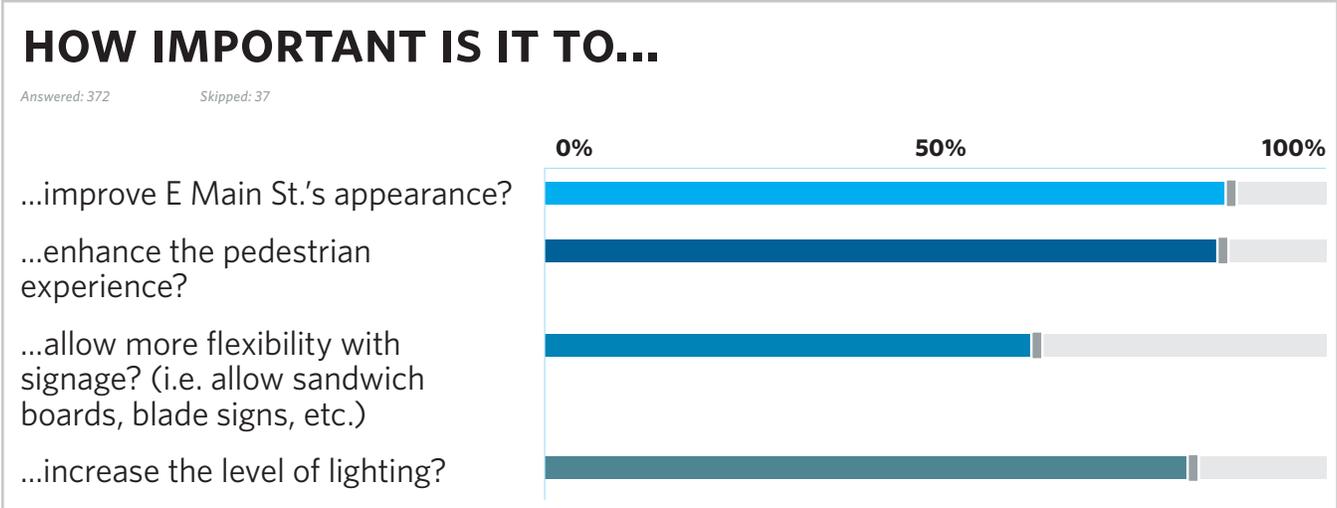
- Additional landscaping
- Planters
- Public art
- Seating
- Seat walls
- Bike racks
- Decorative lighting
- Wayfinding signage
- Special paving
- Temporary and/or permanent parking space parks

Potential Investments to Improve The Marketing Zone

- Storefront beautification (extension of window displays to exterior spaces)
- Operable windows
- Awnings
- Decorative lighting
- Outdoor seating
- Outdoor displays
- Planters
- Blade signs
- Sandwich Boards



Examples of amenitized storefronts, sidewalks, and parking spaces



The ninth question of Survey #2 asked residents about the importance of improving the appearance and experience of Main Street.



Existing Conditions on Main Street: Sidewalks have limited amenities and character.



Proposed streetscape: layered accessories create a vibrant and inviting pedestrian experience.

PEDESTRIAN-FRIENDLY PASSAGES

Downtown Riverhead has a series of mid-block pedestrian passages that connect shops to parking lots, adjacent neighborhoods, and the riverfront. The passages provide critical connectivity but some are narrow, empty, and unwelcoming. The width of passages in Riverhead varies but on average is between about ten and twenty feet wide. As a result, residents say these passages can feel unapproachable or unsafe, particularly at night. Although the width between buildings is unlikely to change, there is potential for buildings to get taller on either side of the passages, which would further exaggerate the canyon-like feeling within.

RECOMMENDATIONS

Simple, small-scale improvements should be considered in order to make existing passages more inviting, including:

- Designing gateways to signal the passage.
- Installing decorative lighting to improve safety at night.
- Create public art like murals and sculptures to add visual interest.
- Add wayfinding signage to help pedestrians navigate to major amenities like the Peconic River, surface parking lots, and more.
- Encourage buildings to have entrances and/or windows that open onto the passage to engage with the public realm.
- Add outdoor seating to provide opportunities for gathering and lingering.
- Incorporate landscaping to enhance the experience.
- Build landscape elements like trellises to reduce the canyon-like feeling.



Passages are not particularly inviting, but are convenient and have great potential

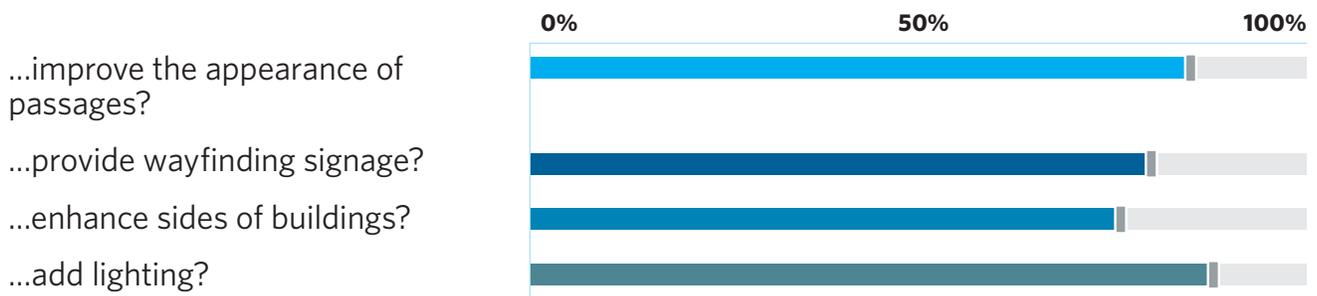


Even narrow passages can implement planters, awnings, and windows facing the passage

HOW IMPORTANT IS IT TO...

Answered: 372

Skipped: 37



The tenth question of Survey #2 asked residents about the importance of improving the appearance and experience of passages.



Wayfinding signage



Landscaping



Landscape elements



Additional decorative lighting



Public art



Building entrance

PUBLIC SAFETY

Throughout public community meetings, online surveys, and stakeholder meetings, the lack of a sense of safety at some times of day was cited as one of Downtown Riverhead's biggest weaknesses. The built environment plays a big part in shaping the perception of safety both day and night, regardless of the relatively low crime statistics. According to residents, major issues are:

- Lack of adequate lighting on sidewalks, in parking lots, and in public spaces.
- Lack of "critical mass" of people on the sidewalks or occupying buildings throughout the day and night to contribute to a sense of accountability in public spaces.
- Obstructed sightlines in passages and around building corners.
- Lack of distinction between strictly pedestrian versus vehicular realms.
- Lack of clear delineation between public and private property.
- Lack of public spaces, outdoor seating, etc. that would promote gathering instead of loitering.
- Lack of maintenance of vacant storefronts and back-of-house areas that creates a disheveled appearance.

RECOMMENDATIONS

- Encourage down-lit lighting on building entrances, on sidewalks, in open spaces, and in parking lots to create a more uniform light level.
- Use curbing and landscaping to direct automobile and foot traffic into controlled, visible areas and to delineate between public and private property.
- Encourage the maintenance of public areas and storefronts to send the message that people care about their downtown.
- Install friendly signage to signal security cameras (i.e. "smile, you're on camera!").
- Improve the passages between buildings by adding lighting, benches, planters, murals, etc. in order to appear occupied and maintained.
- Encourage storefront displays and businesses to accessorize the sidewalk immediately outside the storefront with plants, products, signage, and other decorative pieces to promote a friendly atmosphere.
- Prioritize places to socialize, but not loiter, in order to improve natural surveillance and a "critical mass" of users on the streets and in parks.
- Enforce requirement for businesses to screen trash cans, dumpsters, pallets, and other waste receptacles from public view.

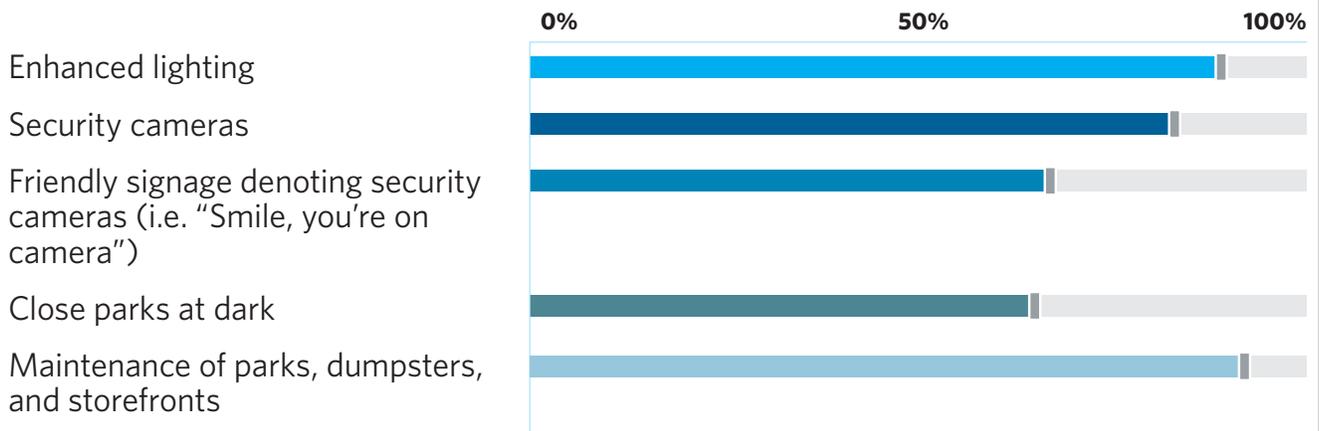


Untidy conditions send the wrong message to visitors

WHAT TECHNIQUES ARE MOST IMPORTANT TO INCREASE THE SAFETY OF PUBLIC SPACE IN DOWNTOWN RIVERHEAD?

Answered: 372

Skipped: 37



In the second survey, residents were asked to select all of the techniques often used to increase the sense of safety; this list of options were informed by feedback provided in community meetings and from the first online survey.

PARKS AND OPEN SPACE

Public parks and open spaces are a vital element in a vibrant, functioning urban area. Providing a literal breath of fresh air, a network of diversely scaled and programmed parks can be used as recreational facilities, gathering spots, walking paths, resting stops, special events locations, and so much more. The area in and around downtown currently features a broad range of open space uses, including the Peconic Riverfront Park, River and Roots Community Garden, East End Arts, John Lombardi Memorial Park, and Grangabel Park, all offering places to stroll, wander, take a seat, hold small gatherings, and even garden. But according to community feedback, these public amenities often lack some of the small but necessary infrastructure to function properly. Some auxiliary uses like playgrounds and specific athletic facilities were requested. Additionally, existing parks do not feel connected within a single network of open spaces; rather, each park feels distinctly separate from other open spaces and even its immediate surroundings.

RECOMMENDATIONS

- Consider methods of visually, physically, or symbolically connecting existing parks as one continuous network, including:
 - Creating signage indicating “Downtown Riverhead Park System.”
 - Adding wayfinding signage to direct pedestrians and cyclists towards other parks and paths/trails to get there.
 - Implement a walking path (to be coordinated with a cycling plan) to physically connect parks.
- Encourage the addition of playgrounds, splash pads, tot lots, and other family-oriented recreational facilities.
- Add trash cans and recycling bins, drinking water fountains, pet waste receptacles, etc.
- Consider adding more benches, picnic tables, gazebos, and other pavilions for lingering and gathering.
- Encourage the planting of trees, flower beds, and other native landscaping elements to add seasonal shade and natural textures.
- Incorporate stormwater management strategies into existing and new parks, especially with attractive solutions including bioswales, retention ponds, rain gardens, etc.
- Encourage parks and open spaces to be designed as flexible spaces, able to accommodate day-to-day leisure activities as well as special events.



Regional parks and open spaces with diverse programming provide pleasant atmospheres



The institutional uses and open spaces within Downtown represent both local and regional draws.



With seating, a mix of hard paving and lawn, and a central location, East End Arts is a well-loved institution with a highly successful open space.



Grangabel Park's sculptures, walking path, and event space are used seasonally.

TOWN SQUARE

According to community feedback, many agree that one of Riverhead's biggest weaknesses is a lack of visual and physical connections between the Main Street corridor and the town's namesake, the Peconic River. The visual connection between the two features is currently limited to the short section of Peconic Avenue, allowing Riverhead's finest natural amenity to be somewhat hidden. Additionally, many residents feel that downtown is missing a large public open space and want a park that is flexible, used year-round for events, and anchors the Main Street corridor as a landmark destination.

A separate but related challenge is a block of mid-century storefronts that have sat vacant for over a decade. These storefronts were originally built to hold department stores, like Swezey's furniture store, that have since moved to Rt. 58, leaving behind large one- to two-story buildings without much architectural character. Located at a critical node on Main Street—across from the landmark Suffolk Theater—these vacant storefronts are a severely underutilized eyesore.

PRELIMINARY DESIGN PRINCIPLES

- Create both a visual and physical connection between Main Street and the Peconic River.
- Utilize infill development to frame the square.
- Facilitate daily programming and activity.
- Create a flexible plaza to host festivals, events, and celebrations.
- Line the square with activated storefronts.

RECOMMENDATIONS

- The Town should engage residents and stakeholders in a dialogue about the design of the new town square.
- The Town should set a target for redevelopment of the town square in the next five years.
- Property owners between Riverview Lofts and Sunny's diner should collaborate with the Town of Riverhead on a unified vision for future redevelopment.
- Active uses should be prioritized.

Note: The Town of Riverhead received an \$800,000 Empire State Department grant to create a new town square connecting Main Street to the river with recreational amenities and a gathering space. Vacant storefronts along Main Street have been purchased by a local developer, the Long Island Science Center, and the Town, respectively. This area should be considered as a prime location for a new town square.



Recently-purchased buildings recommended as the site of a new Town Square

STATUS OF LOTS IN THE TOWN SQUARE TARGET AREA*

- 101 E Main Street (purchased by local developer for mixed-use development)
- 111 E Main Street (purchased by the Long Island Science Center for renovation as their new location)
- 117, 121, 125, 127 E Main Street (purchased by the Town of Riverhead)

*as of publication

DOES DOWNTOWN RIVERHEAD NEED MORE PUBLIC OPEN SPACES? IF SO, WHAT TYPE DO YOU PREFER?

Answered: 1,024 Skipped: 225

WATERFRONT PARK



63% (644)

TOWN SQUARE



51% (525)

TRAILS



29% (298)

NO MORE PUBLIC OPEN SPACE

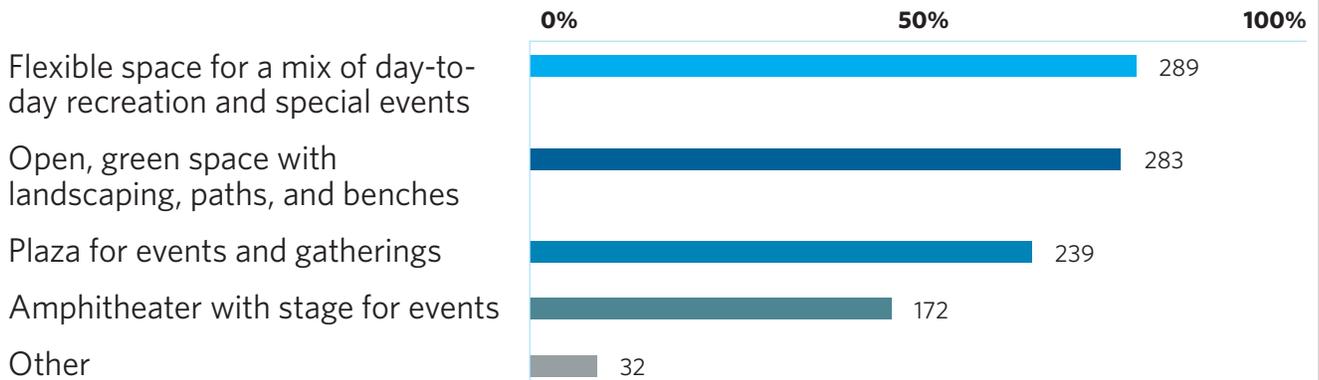


17% (178)

In the first survey, residents were asked if more public open spaces were needed downtown and, if so, what type were desired; written-in responses included recreational spaces, green areas, activities for children and families, public access to waterfront, concert seating for live music, space for public art, kayak launch areas, a sense of safety and destination, and more.

WHAT KIND OF EXPERIENCE SHOULD A TOWN SQUARE OFFER?

Answered: 370 Skipped: 39



In the second survey, residents were asked to check all desirable experiences that a new town square might offer; written-in responses included food trucks, mix of benches, covered picnic pavilions, and open areas, playgrounds and other family-oriented spaces, seasonal retail and market spaces, water features, a carousel, an ice rink, bike racks with access to trails, and more.



This visual illustrates hypothetical design for a town square, framed by retail, a renovated Long Island Science Center and the Suffolk Theatre.



A potential town square provides space for new entertainment, retail, and restaurant space with increased space for outdoor dining and events.



A view of a potential new town square which provides an urban and mixed-use opportunity to connect Main Street and the Peconic River.



A new town square will provide residents and visitors a beautiful outdoor public gathering space which highlights the historic Suffolk Theater.

SIGNAGE

Communicating the use of a building, advertising a service or business, and directing visitors in Downtown are important to promoting safety, celebrating events, and creating a vibrant pedestrian experience. Although the current regulations for signage attempt to reduce clutter on sidewalks and encourage high-quality signs, they result in complicated, restrictive rules that actually discourage any signage at all. Additionally, feedback from the community noted that these strict regulations were rarely enforced. A vibrant public realm is highly dependent on activity on the sidewalk and in front of storefronts, enticing passersby into the stores and acting as advertisement to drivers on the heavily-trafficked Main Street corridor.

RECOMMENDATIONS

- The Architectural Review Board should encourage animating the public realm using signage. Signs design and graphics should be coordinated with the character of the building and the nature of the business within.
- Sandwich boards should be allowed in the DC-1 Zoning Overlay.

Per §301, these signs are currently allowed by the Town without a permit:

1. Temporary window signs
2. Parking signs and traffic regulation signs
3. Farm market temporary signs
4. Theater outer lobby poster
5. Incidental signs
6. Nameplates
7. Historic signs
8. Dedicatory signs
9. Public information signs
10. Parking signs, public and private

Per §301, these signs are allowed by the Town with a permit:

11. Wall signs
12. Roof signs
13. Freestanding signs (including pole and monument signs)
14. Permanent window signs
15. Directory signs
16. Marquee, attached
17. Awnings/canopies

18. Gasoline station signs
19. Soffit signs
20. Projecting signs
21. Nameplate
22. Electronic time/temperature signs
23. Menu board
24. Wall sign
25. Incidental informational window signs
26. Commercial property real estate signs
27. Residential property real estate signs
28. Construction signs
29. Subdivision signs
30. Off-premises directional signs
31. Temporary business signs
32. Temporary special event signs
33. Political signs and posters

Per §301, these are currently prohibited by the Town:

1. Off-premises commercial advertising signs



SIGNAGE DESIGN PREFERENCES

- Allow sandwich boards in the DC-1 Zoning Overlay in order to animate the pedestrian realm
 - Encourage a variety of scales of signage types geared towards drivers (larger scale text, higher up, bold graphics) and pedestrians (smaller scale, closer to the ground, projecting from building, more ornate and detailed graphics)
2. Animated, flashing, moving, rotated, revolving, chasing, oscillating, or blinking signs or devices
 3. Banners, ribbons, pennants, spinners, streamers, balloons, or other such devices
 4. Any sign visible from a public right-of-way that is mounted on a vehicle or trailer
 5. Temporary menu, sandwich board, banners, posters, and other such temporary signs within 50 feet of the public right-of-way
 6. Any signs, including handbills and stickers, affixed to a traffic sign, signal, controller cabinet or supporting structure, fire hydrant, utility pole, bridge, tree, rock, statue, or sculpture
 7. Mobile signs



Examples of well-maintained and appropriately placed signage types throughout the region.

This page is intentionally blank.



**OBJECTIVE DESIGN
STANDARDS FOR
PRIVATE DEVELOPMENT**

OVERVIEW

The standards prescribed in this chapter outline the proper character that Downtown Riverhead’s buildings should seek to embody.

Intent

These standards are intended to capture the essential qualities and key patterns of Downtown Riverhead and translate them into a series of quantifiable guidance for the built environment of downtown. The aspects include parameters typically found in a zoning ordinance such as lot coverage, building height, and setbacks as well as principles that designers, architects, and builders shall follow within the area of Downtown Riverhead outlined in this document. It is the companion piece to a subsequent form-based code that will act as the primary enforcing agent after the adoption of this document. This Pattern Book, and by extension the form-based code, is intended to foster predictable results within a sufficient band of flexibility.

Building Types

The palette of building types within the downtown varies from single-family detached houses to multi-story mixed-use buildings. The chart in this section does not make the distinction between the various building types, but it is understood that certain uses may dictate a different building form. The prescriptions here are more concerned with creating architecture that is contextual and harmonious with downtown. This type of form-based approach to regulating new development will ensure that Riverhead’s vision for the physical form of downtown is realized while allowing market demand to determine the uses.



Examples of preferred streetscape character

Facade Variation

Downtown Riverhead features an eclectic collection of buildings from different periods of history and therefore features a richly varied character. It is vital that buildings respond to this exciting architectural diversity. There are many design techniques that can be employed to achieve this, including:

- Window pattern variations
- Breaks in the roof line
- Vertical articulation of bays
- Setbacks of vertical planes of a building

There is community consensus that, in terms of scale, Downtown Riverhead's older building stock was preferred to the larger mid-century one-story buildings. These historical buildings' front facades do not exceed a width of 60 feet; therefore, new buildings should utilize one of the above techniques to indicate a clear change in design intent at least every sixty feet.



Typical facade width of a building on Main St.



One example of a how successful private development can improve the public realm.

LOT DEVELOPMENT STANDARDS

After engaging with numerous stakeholders, community members, and residents at community meetings, a clear vision for the future of Downtown Riverhead emerged. The architectural character and scale of Main Street are strengths that need to be respected as downtown evolves. But, as the demand to live downtown and develop underutilized parcels grows, there must be realistic, feasible financial return for developers and investors to maintain an interest in downtown. The lot development standards strike the balance between maintaining downtown's character and the need to stay relevant.

At the time of publication, five stories and 80% lot coverage is permitted in the DC-1 Zoning Use District. This document proposes four stories but with 100% lot coverage with a slight stepback on the fourth story. This achieves the two primary objectives stated previously. First, this maintains the small-town character cherished by residents, as exemplified in the image below. Secondly, this maintains approximately the same permitted developable square footage for any lot.

PRIMARY LOT DEVELOPMENT OBJECTIVES

- The existing historical architectural character and scale of Main Street should be respected
- Realistic, feasible financial return for developers and investors must be achievable to offset land and construction costs



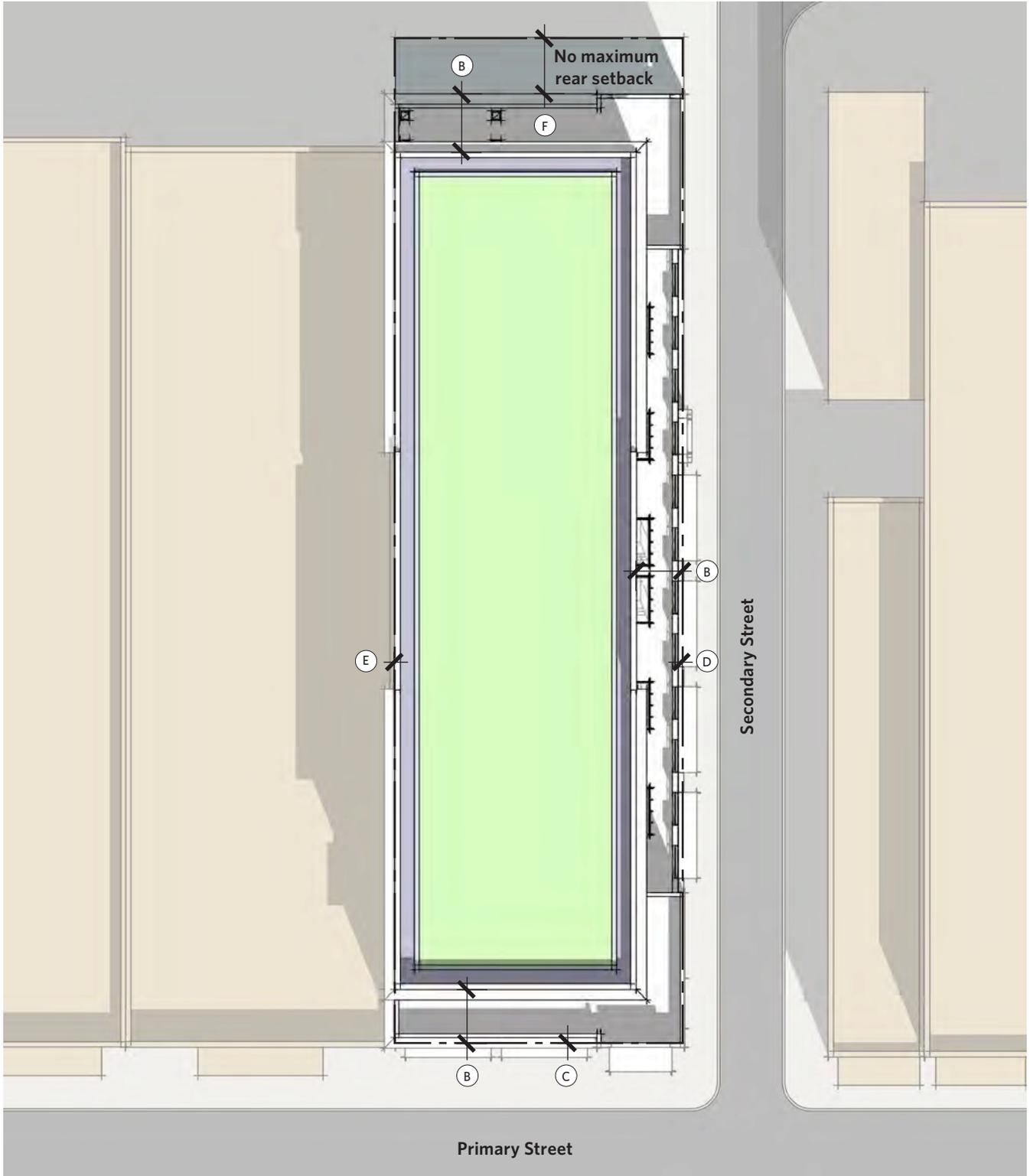
A 4-story building on Main Street appears as a 3-story building to a pedestrian.

DC-1 Zoning Use District Objective Design Standards for Private Development

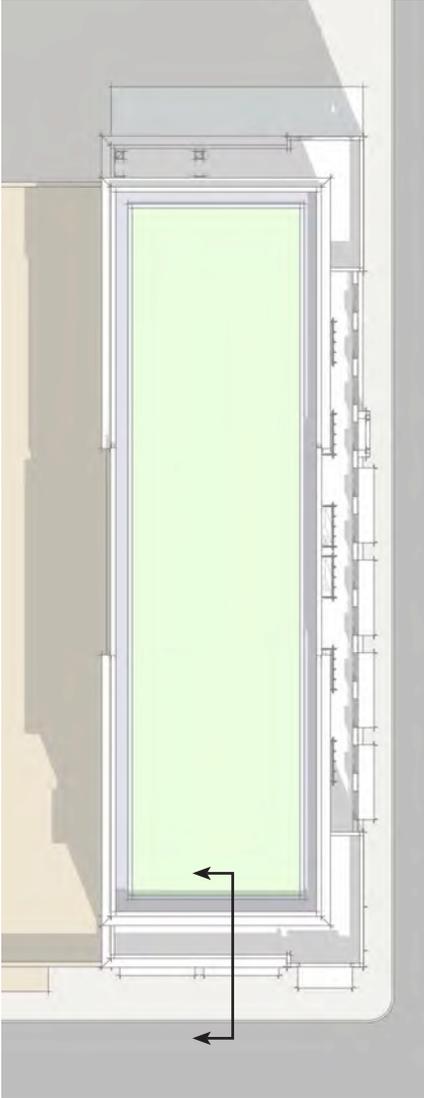
Height	(A) Building Height	4 stories or 50' maximum
Stepbacks	(B) Building Stepbacks	45° above the 3rd story, measured from the top of 3rd floor parapet (or where the 3rd story parapet would be if not included). This applies to addresses along Main Street, the Peconic River, side streets, open spaces, and freestanding-historical structures.
Setbacks	(C) Front Street Setback	Minimum setback is equal to adjacent building(s) setbacks on the same block frontage. 13' minimum from the face of curb encouraged to allow for outdoor dining and/or displays. Maximum 3' offset from adjacent building(s). 13' minimum setback from face of curb where there are no buildings on adjacent lots.
	(D) Side Street Setback	0' minimum
	(E) Side Yard Setback	0' minimum
	(F) Rear Yard Setback	0' minimum
Lot Coverage	Lot Coverage	100% maximum
Floor Area Ratio	Floor Area Ratio	3.5 maximum
Green Roof Coverage	(G) Green Roof Coverage	40% minimum of the lot area
Encroachments	Permitted Encroachments	Signage, lighting, awnings, canopies, bay windows, and balconies may encroach into the front yard and past rights-of-way so long as they maintain an 8' clear space above the sidewalk and extend no more than 2'.
Ground Level Activation	Ground Level Activation	Minimum 20' depth of the ground story facing Main Street and the lots adjacent to streets along the Peconic River shall have an active use.
Parking and Vehicle Access	Parking & Vehicle Access	Vehicle ingress and egress shall be taken from an alley where the condition exists. Parking is not permitted in the first 20' of the ground story facing Main Street and the first 20' adjacent to streets along the Peconic River.
Zero Net Energy	Zero Net Energy	Meeting ZNE standards is strongly encouraged, including the use of solar panels.



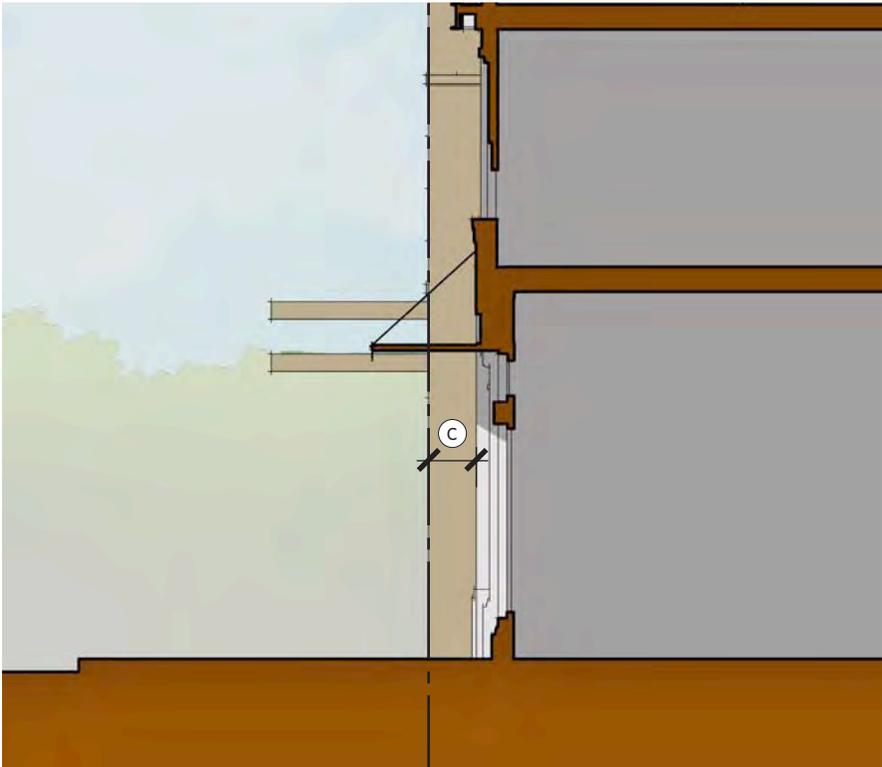
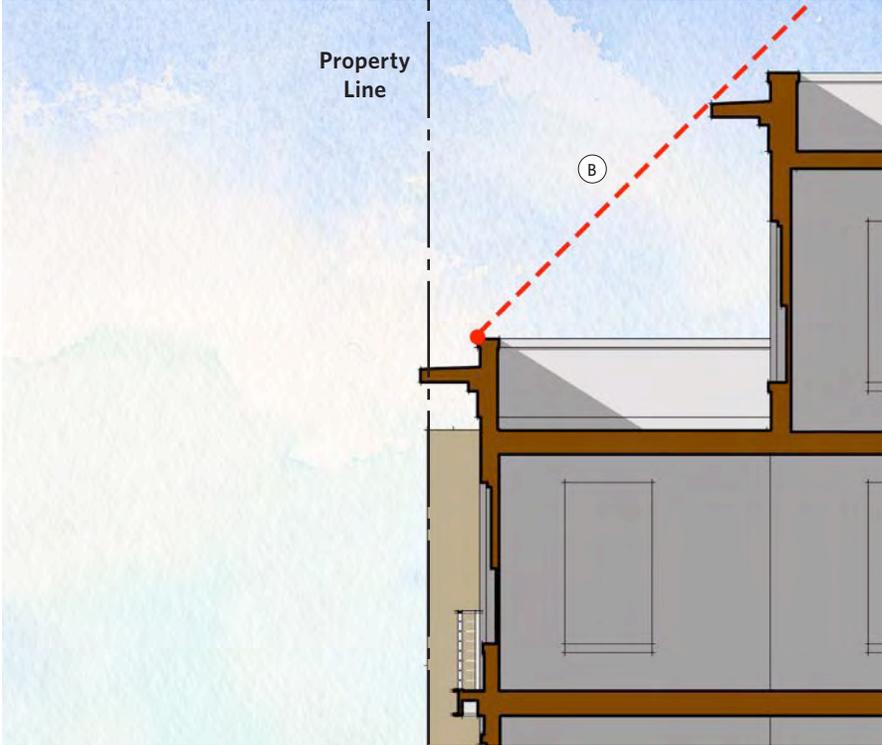
One hypothetical proposal shown above for the DC-1 Zoning Use District.



A lot plan for a hypothetical proposal in DC-1 Zoning Use District.



Key Plan for adjacent building section.



This section illustrates how a building relates to the street.

OBJECTIVE DESIGN STANDARDS FOR PRIVATE DEVELOPMENT

ARCHITECTURAL PRINCIPLES

MASSING

The buildings of Downtown Riverhead are generally simple in massing regardless of use, however three distinct building types do emerge in downtown: background, residential, and icon buildings. Background buildings are the mixed-use buildings with storefronts, typically masonry, that line much of Main Street today. The residential buildings, typically converted houses, are set back slightly further and feature sloped roofs and porches. Finally, icon buildings are those freestanding structures, like churches, banks, and cultural attractions, that are hierarchically the most prominent.



Massing responding to the scale of the street



An example of a background building



An example of a residential building

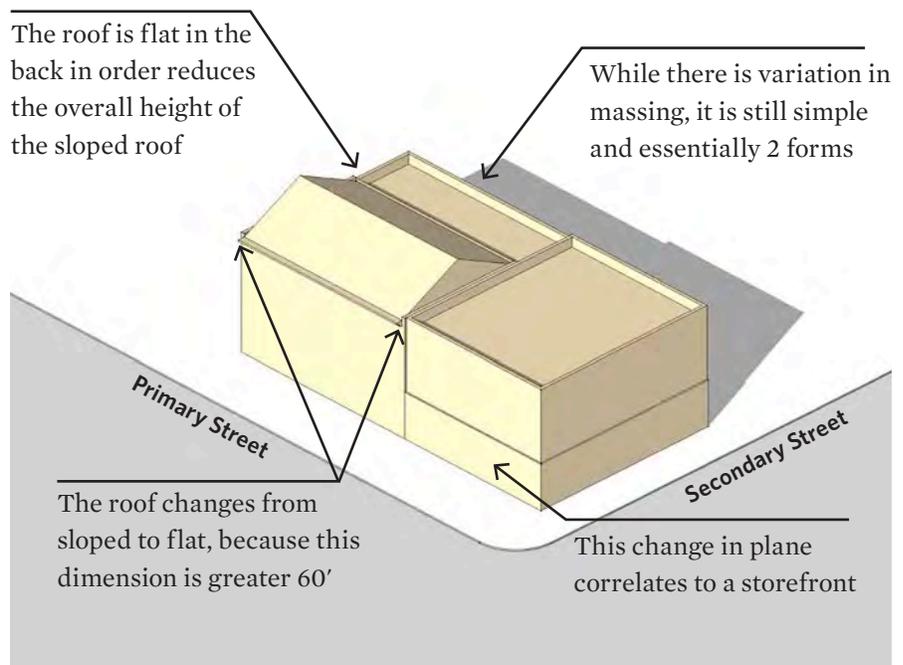


An example of an icon building

Objective Design Standards

- Buildings with a length greater than 60' along any street shall use one of the following architectural elements in order to break down the scale of a building.
 - Bay windows
 - Parapets or distinct roof forms
 - Changes in wall plane
 - Balconies
- Changes in massing or form, if employed in increments of less than 60', shall only be used to differentiate ground-story tenants, emphasize buildings entries, or delineate service areas.
- Changes in wall plane shall be a minimum of 8" to create a sense of depth and shadow.
- Mansard Roofs are prohibited.

Example



COMPOSITION

Buildings in Downtown Riverhead typically have a clear base, middle, and top with punched window openings above the ground floor. Punched window openings or windows set within a wall, not walls of glass, are typical of downtown. Window patterns should reinforce the massing of the building and relate to other architectural elements. The objective design standards below describe how these goals can be achieved.



A storefront acts as a base for this building



Horizontal banding divides the building in thirds



Entries can be emphasized with an alcove

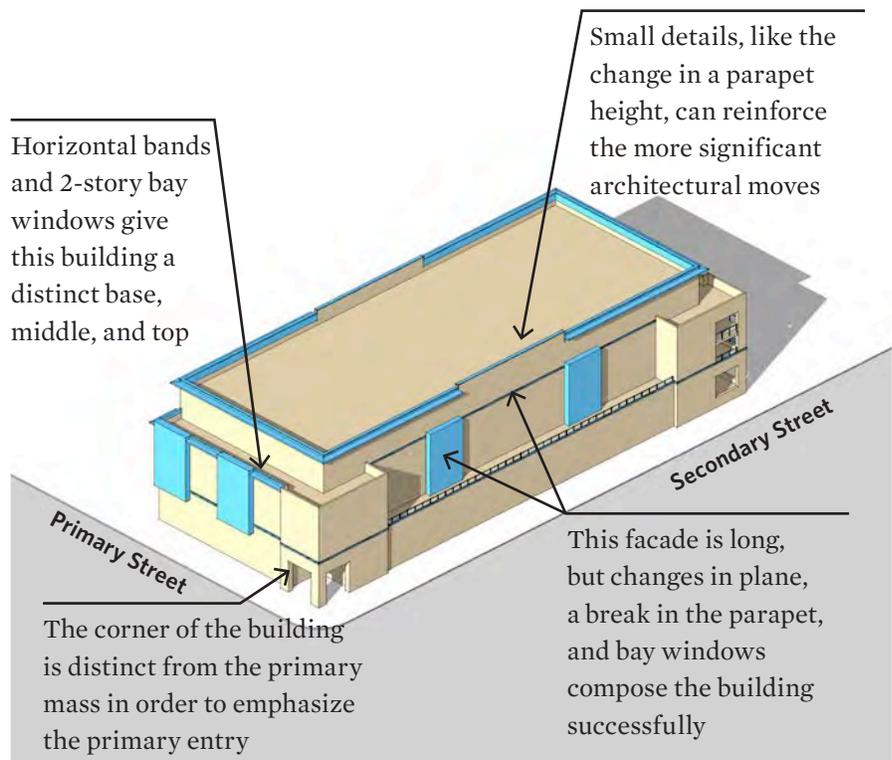


Each storefront is a unique material

Objective Design Standards

- Buildings shall have a distinct base, middle, and top. One of the following architectural elements shall be used to achieve this.
 - Material changes
 - Horizontal Banding
 - Distinct window patterns
- Changes in material shall only occur at inside corners.
- Entries shall be emphasized with awnings, signage, a change in plane, or a change in the window or door pattern.
- Blank, featureless walls shall not face primary or secondary streets.
- Above the first story, walls shall not be composed entirely of glass.

Example



STOREFRONTS

A successful storefront blurs the distinction between the public sidewalk and the indoor uses. Characteristic elements include large transparent display windows with a solid panel below and clerestory windows above, recessed front entries, and exterior awnings and signs. A successful building balances a need for an overall, coherent building design with a tenant's desire to have a storefront tailored specifically to their business. These objective design standards emphasize the first 14 feet of a building's elevation: the part of the building that is most highly visible to a pedestrian.

Objective Design Standards

- A storefront shall have a door that accesses the primary street.
- Storefronts shall utilize the full height of the ground-floor facade or a minimum of 14' in height, whichever is greater.
- Glazing shall constitute a minimum of 70% of the facade to the top of the first floor ceiling or 14', whichever is greater, for non-residential uses.
- Storefront entrances shall be clearly distinguished from those serving floors above.
- Multiple storefronts within the same building shall be visually compatible in terms of scale and alignment, but distinguishable from one another using windows, color, signage, or awnings.
- Storefronts shall maintain a repetitive bay system and only break the rhythm for an entry.
- Lighting shall be mounted and down-firing in order to prevent dark sky intrusion.

Examples

This successful storefront has a facade with glazing composing over 70% of its total area



This percentage of glazing produces achieves the objective design standards successfully



This storefront spans the entire first floor and blends traditional storefront elements with a modern structural system.



WINDOWS & DOORS

Blank, featureless walls and walls composed entirely of glass should not face any primary or secondary street, passage, or open space. These design approaches are not consistent with Downtown Riverhead’s character. Doors in residential units are encouraged in order to provide access to private outdoor space. Additionally, as buildings become more environmentally sustainable, limiting the amount of glazing on upper floors is increasingly important. Punched openings are energy efficient, cost effective, and contextual.



Large storefront windows are inviting

Objective Design Standards

- Window patterns and doors shall be consistent with the style of the building.
- Windows shall not be flush with the exterior wall surface, but inset a minimum of 1”.
- Storefront windows shall be larger near the street, and all other smaller and regularly composed on upper floors.
- Windows shall not constitute the entirety of a facade.
- Window patterns shall relate to storefronts below as well as the roof forms above.
- Window patterns of single or grouped windows shall emphasize vertical proportions.
- Doors shall be located in prominent locations, and should have lintels, trim, transoms, and other elements appropriate to the scale of the opening and the building’s architectural style.
- Doors for commercial uses shall be a minimum of 70% glazing.
- Residential units shall have at least one operable window.

Examples

The storefront is composed of several doors which improves the quality of the pedestrian experience



Upper floor windows are not flush with the face of the brick, adding shadow lines and interest



This window successfully connects the use to the sidewalk



MATERIALS

As discussed in the following section entitled "Architectural Styles," the history of downtown spans multiple periods of architectural influence and therefore reinforces what can be seen today: an eclectic Main Street. While more than a single style is appropriate for downtown, this does not imply that all materials are appropriate for downtown. Regardless of style, materials of new buildings should be part of the existing palette and vocabulary of downtown. The material of buildings across a street and immediately adjacent to a proposed building are critically important. Authentic materials, or materials that age well and are common to Riverhead, and are the tradition in the area and are best suited for its appearance in the future.

Building Elements

The following list of permitted materials is intended to reflect the current building materials found in downtown, while also providing for flexibility within contemporary building techniques and constantly fluctuating market conditions. Use of one of the materials on this list only applies to the parts of buildings visible from the sidewalk of a primary or secondary street. The intent is to utilize those materials that are more common to the area, age gracefully, and enhance the experience of those that work, live, or visit downtown.

Foundations

Foundations shall be stone, brick, or parged concrete with a smooth finish.

Walls

Walls may be one of following materials listed below, however masonry, and in particular brick, is more prevalent in Downtown Riverhead than many other downtowns of eastern Long Island. Walls may be:

- Siding: Wood, synthetic wood, or smooth fiber cement siding
- Shakes: Wood, synthetic wood, or fiber cement siding
- Panels: Synthetic or fiber cement panels
- Masonry: Extruded or molded brick with minimal variation in color, stone, cast stone, synthetic stone only if in a horizontal pattern, or stucco

Windows and Doors

Windows shall be square or vertical in proportion and may be fixed, single-hung, double-hung, casement, awning, or hopper. The frame should be wood, wood-clad, cellular PVC, steel, or aluminum. Doors should be aluminum, steel, vinyl, painted or stained wood or fiberglass. Glazing shall be clear glass.



Brick common in Riverhead



Brick is common in Riverhead



Wood trim and clear glazing on a storefront

Eaves and Parapets

Eaves should be a consistent depth on all sides of a particular building and parapets should effectively hide the roof, mechanical equipment, vents, and other service elements of the building. Particular detail should be paid to the underside of the eave as it is highly visible to the pedestrian below on the narrow sidewalks of Downtown Riverhead.

- Trim: Wood, synthetic wood, or cellular PVC. Frieze boards shall be flush with corner boards.
- Brackets: If used they should be wood, synthetic wood, or cellular PVC and match the material of the frieze board. They shall extend to the face of the eave.

Columns

Columns, piers, and pilasters should be consistent with the style of the building and should be wood, fiberglass, steel, or aluminum.

Roofs

Most buildings along Main Street should have a flat roof and a parapet and therefore will not be visible from frontage. When the primary roof is sloped it should be asphalt shingle, slate, or synthetic slate. In addition to the materials listed above secondary roofs, such as porches, stoops, and bays, may be 5-V metal crimp roof. It should be standing seam with flat panes between primary ribs with no striations.

Building Accessories

Elements, like gutters and balconies, that vary broadly with style are listed in this section.

- Gutters and downspouts: They should be strategically placed to be minimally visible from a front-on view of the building, but when used they should be placed to cover control or expansion joints in a masonry veneer facades or near corners. They should be painted to match the surface to which they are affixed. They may be cooper, primed or prefinished metal, or zinc.
- Balconies: They may be steel, iron, or wrapped in one of the trim materials listed in the eaves and parapets section.
- Dormers: They should be wood, synthetic wood, or smooth fiber cement siding when. They may only be masonry if they are a continuation of the wall below in the same material.



Permitted eave and soffit materials



Painted wood columns shown above



Materials for building accessories vary widely

ARCHITECTURAL STYLES

Downtown Riverhead’s eclectic, dynamic architectural appearance stands out from other towns of Long Island.

A stroll down Main Street quickly paints a unique image of downtown, distinct from its East End neighbors such as Southampton, Sag Harbor, or Greenport. The story of development in Riverhead is rooted in buildings designed in styles reflective of the times in which they were built; from the high ceilings and intricate brickwork of the turn of the century, to the wider storefront and sleek materials of the mid-twentieth century, to the boxy windows and siding of the last few decades.

Downtown Riverhead experienced development in three major periods, resulting in the diverse, eclectic environment seen today. Residents see this variety as a strength that sets Riverhead apart from other Long Island towns. Future builders, developers, architects, and designers should respect this rich history and should pay tribute to and design within the historical and traditional parameters of these three styles: Pre-World War II, Post-World War II, or Contemporary architecture. Consistency with one of these three styles is included as part of the objective design standards. In no case should new buildings be a mash-up of different styles.



CONCEPTUAL FRAMEWORK

- The use of a building determines whether it is a monumental “icon” building or a “fabric” building (beyond style); these guidelines are designed to provide a framework for fabric buildings while accommodating more high-style icon buildings.
- Architectural styles existing and preferred for Riverhead include:
 - Pre-World War II
 - Post-World War II
 - Contemporary



One example of a Pre-World War II style



One example of a Post-World War II style



One example of a Contemporary style

OPN Architects

PRE-WORLD WAR II

Characteristics

Architecture predating World War II includes a wide range of independent styles, such as Victorian, Neoclassical, and Colonial Revival, however there are many elements that transcend style and reflect the manner in which buildings were constructed. Walls are load-bearing and the proportions of openings reflect this structural necessity. Cornices are elaborate and follow the prescriptions of the classical orders, although sometimes in a simplified manner. Windows on upper floors are often double-hung and have smaller panes of glass with true divided-lite muntins.

Walls

Brick, in the Common, English, or Flemish bond patterns, often includes a parapet with a stone cap. Siding and shingles were used more often for freestanding buildings, like churches and private residences. Regardless of the material chosen, buildings rarely employ multiple materials on a single facade, except for bay windows or storefronts.

Openings

Windows on upper floors are often double-hung and have smaller panes of glass with true divided-light muntins. Excluding the storefronts, glazing often makes up less than 40% of the facade. At the ground level, storefronts feature nearly all glazing; however, transoms are a common part of the storefront system due to the limitations of producing large sheets of glass at that time.

Eaves and Cornices

Ornament is more common in architectural styles that predate World War II, but it is generally reserved for window surrounds, storefronts, and cornices. The masonry openings of Main Street, however, are less elaborate. Instead, the most articulated aspects of the architecture are seen in storefronts' brackets, dentils, and moldings.



Elements of Pre-World War II architecture

POST-WORLD WAR II

Characteristics

Architecture built after World War II is distinct from the architecture of a few decades earlier largely due to the divorcing of the wall from the structure. Structural elements become expressive and metal elements are often exposed. Ornament is not found in cornices, window trim, or columns as in Victorian or Colonial architecture. Proportions are horizontal and opening patterns reflect this structural freedom.

Walls

Masonry is common, but used strictly as a veneer. Veneer does not attempt to express its structural qualities and joints are minimal or disguised. The form of the totality of the building itself is often more important than the individual elements that constitute the wall. Siding and shingles are not typical of this style on mixed-use buildings.

Openings

Windows are square or horizontal in proportion and are rarely subdivided into smaller panes of glass. Storefronts are typically part of a curtain wall with little distinction between doors or windows. The amount of glazing between the ground floor and upper floors is relatively similar.

Eaves and Cornices

Eaves and cornices are extremely simple and clean; however, they are often deep or almost flush with the facade in order to appear either distinct from the wall or a continuation of the wall. They may be two feet or more in depth and sit relatively close to the top of the window pattern. The fascia may be aluminum or exposed metal and is often a neutral color.



Elements of Post-World War II architecture

CONTEMPORARY

Characteristics

There is no single style that can be called contemporary; rather, the term literally means “of its time.” Within this category are stylistic subsets such as conceptual, neoteric, and referential. Most contemporary buildings are designed to clearly communicate internal program through structural grid sizing and facade rhythms, following the modernist concept of form follows function. Program also determines the significance of the building within its context; for example, a multi-family residential building is considered a background building and should blend naturally into its context, whereas a museum is considered an icon and should stand out from its context.

- Conceptual architecture is generally reserved for icon buildings. Inspired by sculptural forms, these buildings generally employ few materials, parametricism, and irregular geometries to appear unique and futuristic.
- Neoteric architecture refers to a straight-forward background style, where internal program prescribes a typical structural grid dimension, which in turn is reflected in simple punch openings in the facade. Multiple materials and bright colors are often used to create interest on a street. A more porous ground floor is typical.
- Referential architecture describes buildings built in the 21st century that borrow details, materials, and proportions from Pre-World War II styles. A variety of materials are often used to create the illusion that a single building is actually broken up into multiple buildings. Simplified eave and trim details are common.

Walls

Brick, siding, and panels are used interchangeably on a single facade. As a way of distinguishing the base from the upper floors or a corner element from the main mass of a building. A change in material is a way of articulating a facade, because ornament is not as heavily used.

Openings

Openings are regular but are often square or vertical in proportion. They often alternate between paired or grouped windows and are a reflection of the floor plans. The layout of a space directly relates to the opening on a facade.

Eaves and Cornices

Eaves and cornices of contemporary styles strikes a balance between the highly ornate cornices of styles before World War II and the overly simplified fascias found after World War II. The cornices are typically at least 12” deep and may be either a single fascia or 2-part fascia beneath the soffit.



Fitzgerald Associates



Design Collective



Heller Manus Architecture

Elements of Contemporary architecture

This page is intentionally blank.



APPENDICES

AM
PILATES REFORM
STUDIO

YOGAHOLOG
Vegan Studios

APPENDIX A: GLOSSARY

Industry terms used throughout this document are defined below.

- **Active Use**

In this document, the term *active use* refers to a use that is used or intended to be used for human habitation or service to the public.

Permitted uses include: retail stores, banks, personal service businesses, indoor public markets, art galleries and studios, museums, libraries, aquariums and other cultural attractions, restaurants, cafes, bakeries with retail sale on premises, banquet facilities, specialty food stores, ice cream parlors, theaters, cinemas, professional offices (except for veterinary offices), schools (including business and secretarial), places of worship, and residential entrance lobbies and private indoor amenity space.

- **Bump-Out**

The term *bump-out* (also commonly referred to as a bulb-out, curb extension, elephant ear, curb bulb, curb bulge, or nib) refers to an extension of the sidewalk (in elevation and material) into the parking lane that is often used as a traffic-calming measure at corners and mid-blocks.

- **Contemporary**

In its strictest definition, *contemporary* means "marked by characteristics of the present period" (Merriam-Webster). In current architectural discourse, *contemporary* generally refers to buildings designed and constructed in the twenty-first century. This is not to be confused with the term *Modern*, which refers to the architectural style common in the mid-twentieth century.

- **Fenestration**

The term *fenestration* refers to the arrangement of windows and doors (which are often grouped together and called "openings") on the outside elevations of a building.

- **Floor Area Ratio (FAR)**

The *floor area ratio*, commonly abbreviated as FAR, refers to the relation between the total amount of usable floor area in a building compared to the total area of the lot on which the building stands. This ratio is determined by dividing the gross building floor area by the total lot area.

- **Parapet**

A parapet or parapet wall is a low, protective wall along the edge of a roof or balcony and is a functional building element that, on older buildings, is often decorated and enforces a definitive top visual edge for a building.

- **Placemaking**

The term *placemaking* refers to the process of creating quality places in which people want to live, work, play, and learn. Special events programming and active uses constitute placemaking efforts in public spaces.

- **Primary Street**

In this case, a *Primary Street* refers to arterials; in Downtown Riverhead, these include Peconic Avenue, Roanoke Avenue, and Main Street.

- **Receiving District**

According to Town code, a *receiving district* refers to "one or more designated districts to which development rights or Pine Barrens credits generated from one or more sending areas may be transferred, and in which increased development is permitted to occur."

- **Resiliency**

The term *resiliency* is used throughout this document and offers a more comprehensive alternative to the often-used term *sustainability*. Resiliency shifts focus from an intent to maintain an environmentally-friendly status quo and instead favors a holistic re-thinking of design, construction, and maintenance that actively works with nature and natural systems and focuses on longevity of humanity and nature.

- **Right-Of-Way (ROW)**

A *right-of-way*, commonly abbreviated as ROW, refers to the space between private property lines, often containing roadways and sidewalks owned and maintained by a local or regional government.

- **Secondary Street**

In this case, a *Secondary Street* refers to any street that is not considered a *Primary Street*. In other words, any other public right-of-way besides Peconic Avenue, Roanoke Avenue, and Main Street.

- **Sending Area**

According to Town code, a *sending area* refers to "one or more designated districts to which development rights or Pine Barrens credits generated from one or more sending areas may be transferred, and in which increased development is permitted to occur."

- **Transfer of Development Rights (TDR)**

A *Transfer of Development Rights*, commonly abbreviated as TDR, is a zoning approach used to protect land with conservation value by redirecting development that would otherwise occur on this land (called the "sending area") to an area planned to accommodate such growth and development (called the "receiving district"). TDR programs financially compensate landowners of sending areas for choosing not to develop some or all of their land. The land is permanently protected through a conservation easement or restrictive covenant. In order to incentivize this program, the development value of the receiving area is, in turn, enhanced by allowing for new or special uses, greater density or intensity, flexibility with zoning areas and bulk standards, increased floor-area allowances, increased building-height allowances, and maximum

impervious lot coverage allowances. TDR programs can be effective tools for land conservation, but a successful TDR policy requires a consistent long-term approach that benefits both the sending area landowners and the receiving area developers. According to Town code, TDR refers to “the process by which development rights or Pine Barrens credits are transferred from a lot or parcel located in any sending area to another lot or parcel located in one or more Receiving Districts.”

- **Wayfinding**

The term *wayfinding* refers to information systems that guide people through a physical environment and enhance their understanding and experience of the space (Society of Experiential Graphic Design). This is not limited to simply signage; comprehensive wayfinding strategies often include maps, symbols, colors, paths, etc. Increasingly, mobile applications and other digital technologies are employed.

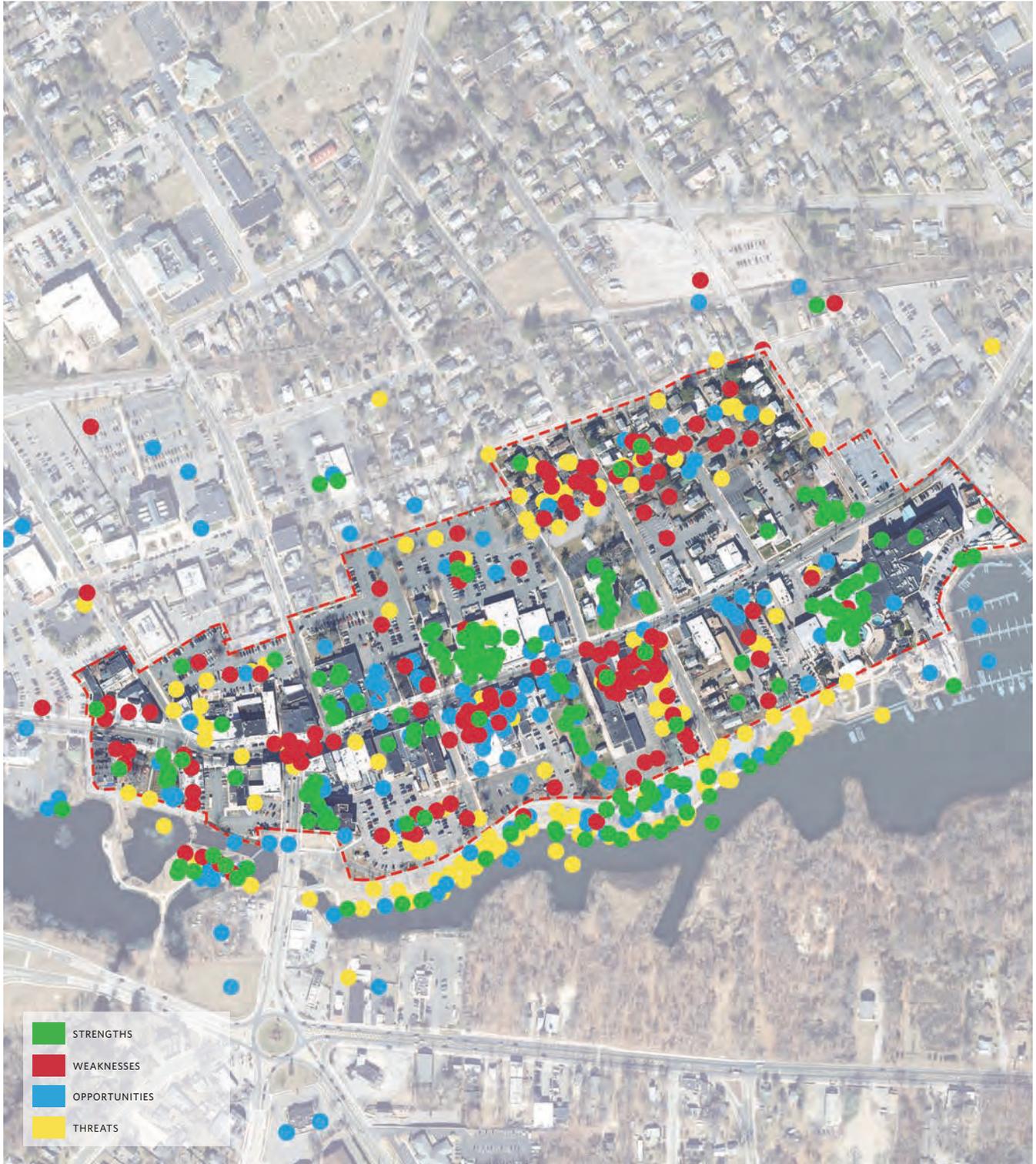
This page is intentionally blank.

APPENDIX B: SWOT ANALYSIS

Listening to residents' impressions of Downtown Riverhead informs the direction of further community engagement and decision-making.

The first step of any community-led design process is establishing common ground. Hosting multiple meetings amongst major stakeholders and community members, reviewing the upcoming engagement processes, and listening to residents' lived experiences of a place is a necessary step in building consensus. The dot exercise is one exercise that quickly communicates a community's impression of their town in a visual medium. In each public and stakeholder meeting, the community was asked to mark the strengths, weaknesses, opportunities, and threats in the downtown DC-1 zoning district using green, red, blue, and yellow stickers, respectively. Combining all these stickers digitally starts to paint a picture of the community's impression of Downtown Riverhead.

PUBLIC COMMUNITY MEETING: NOVEMBER 14TH, 2019



STRENGTHS (11-14-2019)



WEAKNESSES (11-14-2019)

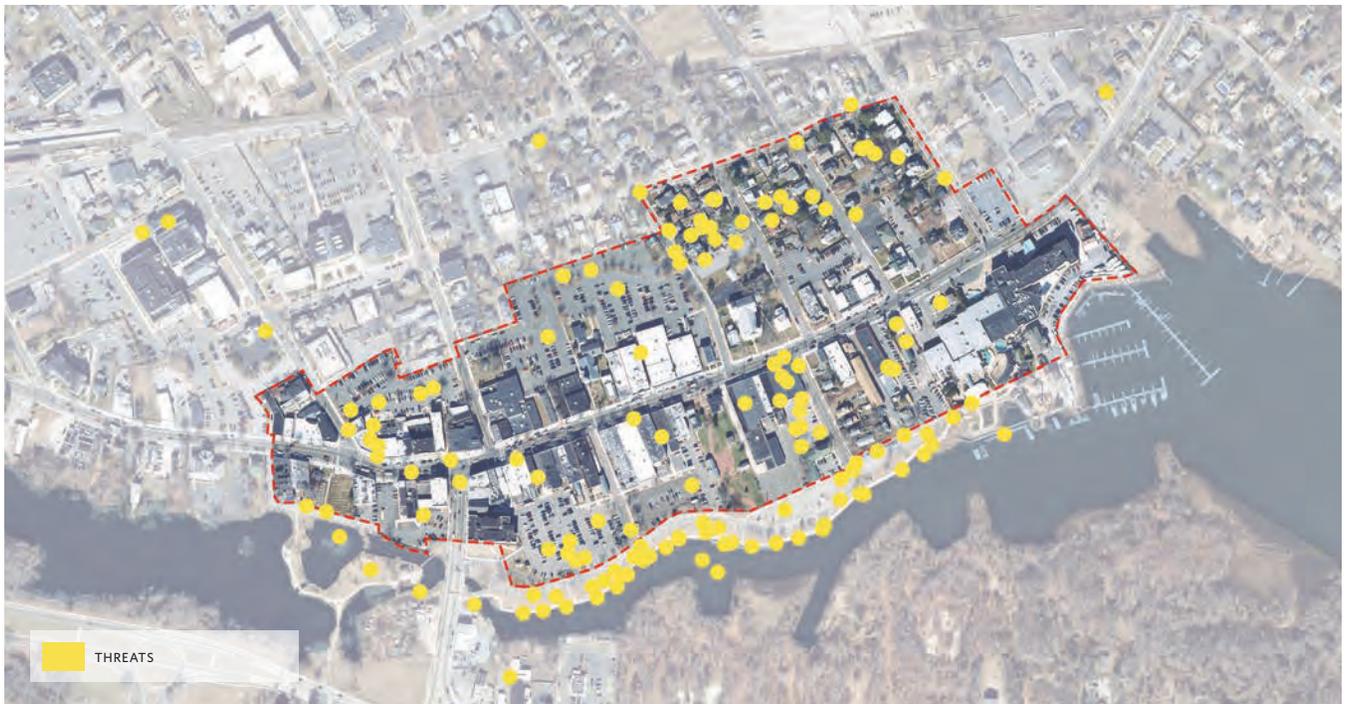


OPPORTUNITIES (11-14-2019)



APPENDICES

THREATS (11-14-2019)



APPENDIX C: ONLINE SURVEYS

Throughout the Listening Phase, two online surveys sought to broaden the scope of community feedback on major issues.

Throughout the Listening Phase, two online surveys were released on the Town of Riverhead’s website in order to gather feedback from as wide an audience as possible. Each survey sought input from the community at a different stage of the project and both informed the structure and content of the Pattern Book.

The first survey was released from January 30th through February 14th, 2020 and collected 1,249 responses. This survey was focused on confirming information gathered from the first round of public meetings held in November 2019. Questions were categorized into three main categories:

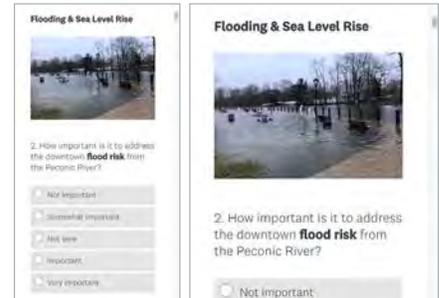
- Strengths, Weaknesses, Opportunities, Threats (SWOT Analysis)
- Buildings & Land Use
- Major Themes and Issues

Each question was worded broadly and the majority provided an option to write in a response in order for contradictory or previously-unheard opinions to be heard. The survey link concluded with another text box and email address for any questions, concerns, or comments that should be addressed.

The second survey was released from June 1st through June 29th, 2020 and collected 409 responses. This survey was focused more specifically on images and sought specific preferences between potential solutions for various issues, as determined by the second round of public meetings held in March 2020 and the previous survey. This survey was divided into two sections:

- Your Preferences
- Architectural Character

Unlike a traditional visual preference survey, the wording of these questions was intentionally focused on broad policy issues rather than specific buildings; the exception to this was the Architectural Character section. These questions were intended to avoid placing value on program and more focused on style by asking similar questions in multiple ways, using examples of buildings from both Riverhead and other Long Island towns.



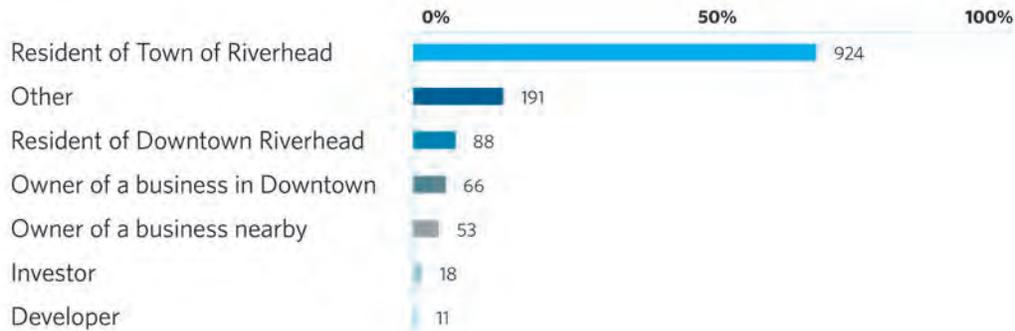
SURVEY BACKGROUND

- Surveys were created through SurveyMonkey
- All responses to the survey were anonymous and limited to one entry per IP address
- Surveys were compatible for viewing on computer, tablet, and phone screens
- About 75% of respondents of both surveys identified themselves as residents of the Town of Riverhead
- Each survey took about 10 minutes to complete and had about 85% completion rates
- The results of each survey were published on the Town website for public review

SURVEY 1: RESULTS

1. WHICH OF THE FOLLOWING BEST DESCRIBES YOUR RELATIONSHIP TO DOWNTOWN RIVERHEAD?

DOWNTOWN RIVERHEAD PATTERN BOOK



***Specified Responses Included: "Routinely shop/travel in Riverhead," "Go for shopping and restaurants," "Consumer, church member, patient of two physicians," "Grew up in Riverhead, often revisits," "Member of East End Arts Council," "Was born and raised in Riverhead," "Concerned neighbor," "Live outside of town but close and go frequently," "I walk there all the time with my kids," etc.

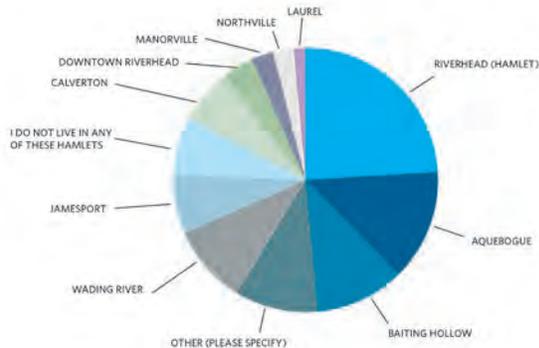
DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

APPENDICES

2. IF YOU LIVE OUTSIDE OF DOWNTOWN, WHICH HAMLET DO YOU LIVE IN?

DOWNTOWN RIVERHEAD PATTERN BOOK

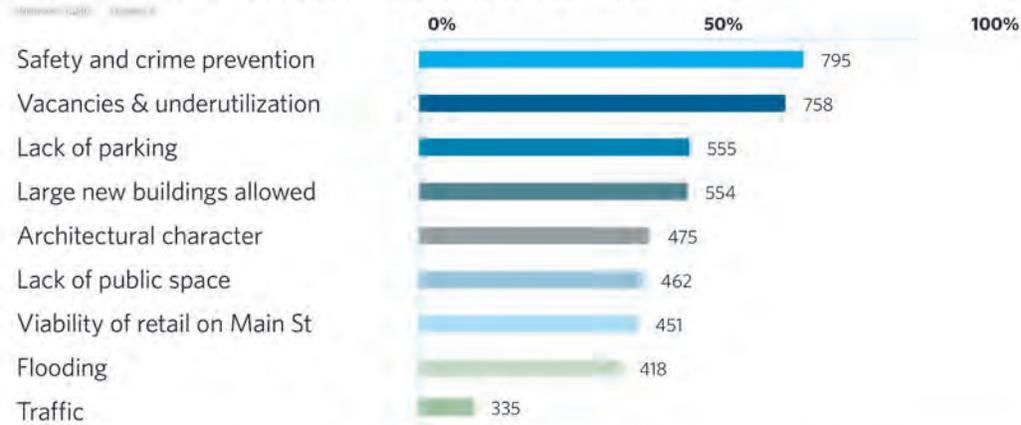


***Specified Responses Included: "Flanders," "Riverside," "Northampton," "Hampton Bays," "Eastport," "Southampton," "Brookhaven," "East Mariches"

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

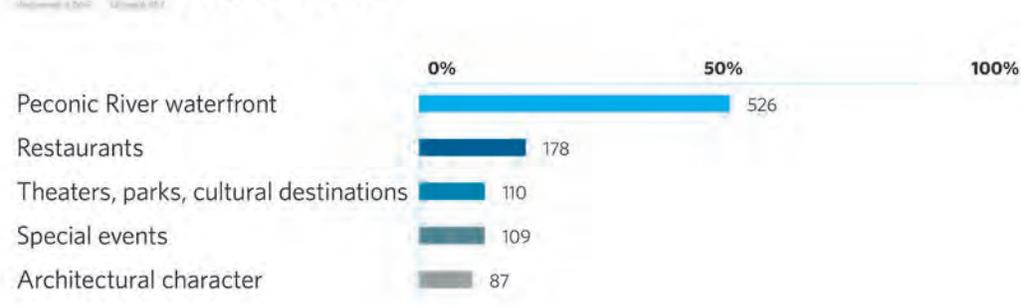
URBAN DESIGN ASSOCIATES

3. WHICH OF THE FOLLOWING BEST DESCRIBES YOUR PRIMARY CONCERN(S) FOR THE FUTURE OF DOWNTOWN RIVERHEAD? CHECK ALL THAT APPLY.



Special Responses included: "Affordable restaurants and stores," "No more apartments or inns," "Blocking view of waterfront," "Need spaces for youth," "Lack of quality public space," "Absence of a village green behind Main Street," "Loss of community feeling," "Lack of visual access to the river"

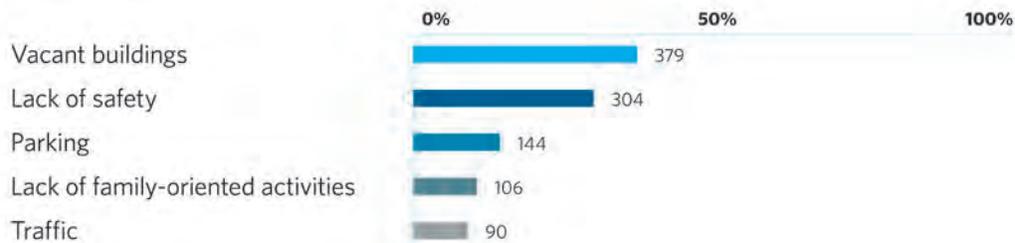
4. WHAT ARE DOWNTOWN RIVERHEAD'S BIGGEST STRENGTHS?



****Our graph shows the percentage of respondents who voted that option as the biggest strength. These options were the top five responses discussed in the first Public Forum, hosted on November 14th, 2019.*

5. WHAT ARE DOWNTOWN RIVERHEAD'S GREATEST WEAKNESSES?

QUESTION 5 OF 10



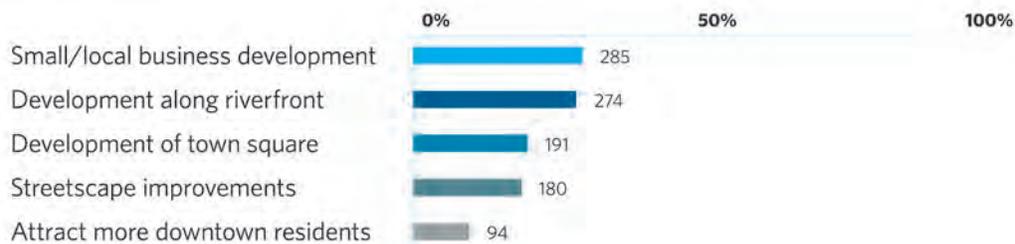
***Bar graph shows the percentage of respondents who voted that option as the greatest weakness. These options were the top five responses discussed in the first Public Forum, hosted on November 14th, 2019.

DOWNTOWN RIVERHEAD PATTERNDRAWN | DOWNTOWN RIVERHEAD | 2019

URBAN DESIGN ASSOCIATES

6. WHAT ARE DOWNTOWN RIVERHEAD'S GREATEST OPPORTUNITIES?

QUESTION 6 OF 10



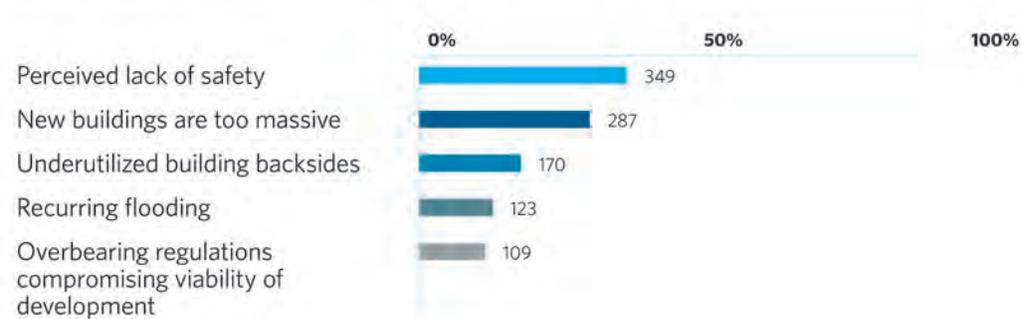
***Bar graph shows the percentage of respondents who voted that option as the greatest opportunity. These options were the top five responses discussed in the first Public Forum, hosted on November 14th, 2019.

DOWNTOWN RIVERHEAD PATTERNDRAWN | DOWNTOWN RIVERHEAD | 2019

URBAN DESIGN ASSOCIATES

7. WHAT ARE DOWNTOWN RIVERHEAD'S BIGGEST THREATS?

November 6, 2019 | 10:00 AM - 11:00 AM



***Bar graph shows the percentage of respondents who voted that option as the biggest weakness. These options were the top five responses discussed in the first Public Forum, hosted on November 6th, 2019.

URBAN DESIGN ASSOCIATES

URBAN DESIGN ASSOCIATES

8. ARE THERE ANY STRENGTHS, WEAKNESSES, OPPORTUNITIES, OR THREATS WE MISSED?

November 6, 2019 | 10:00 AM - 11:00 AM

STRENGTHS

- Dedicated owners and operators of businesses
- Beautiful town that has so much potential
- Rural small town persona
- Arts and cultural groups that can work together
- Suffolk Theater
- Arts & music, festivals
- Gateway to North Fork
- Historical buildings
- Natural beauty of Peconic River and wetlands

WEAKNESSES

- Crime, don't feel safe
- Lack of code enforcement
- Building large structures blocking the character of downtown
- Lack of parking
- Lack of shops does not attract visitors
- Underutilized waterfront
- Empty buildings
- Dark alleys

OPPORTUNITIES

- Movie theater, stores, parks, something for kids
- More music and arts
- Space is available for restaurants and businesses
- Improve the green space and make Riverhead a safer place to walk
- Restaurants, nighttime entertainment
- Grant to create public space on the riverfront
- Focus on small businesses

THREATS

- Negative perception throughout the region
- Big chain stores
- Crime
- Recurring floods
- Slow permit process
- Lack of police presence
- Vacant buildings

URBAN DESIGN ASSOCIATES

URBAN DESIGN ASSOCIATES

9. DOES DOWNTOWN RIVERHEAD NEED MORE PUBLIC OPEN SPACES? IF SO, WHAT TYPE DO YOU PREFER?

Answers: 1,021 Skip question

WATERFRONT PARK



63% (644)

TOWN SQUARE



51% (525)

TRAILS



29% (298)

NO MORE PUBLIC OPEN SPACE



17% (178)

***Specified Responses Included: "Benches on the sidewalks," "Need to clean up the existing park and make it safer," "Recreation/pickleball courts," "Need to feel more safe doing anything after dark," "Green area with opportunities for safe and fun congregation of children and families," "Playground and baseball field," "More public access to the waterfront," "Extend the riverfront boardwalk/parkland westward as much as possible," "Concert seating for summer live music," "Space for public art," "Bike friendly," "Kayak launch areas," etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

10. HOW IMPORTANT DO YOU THINK IT IS TO BUILD A TOWN SQUARE ALONG MAIN STREET?

Answers: 255 Skip question



The average percentage of respondents' ranking was 62%

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

11. WHAT TYPES OF USES ARE MISSING FROM DOWNTOWN RIVERHEAD? CHECK ALL THAT APPLY.

Answers: 1,022 Slipped: 217

RETAIL



58% (602)

ENTERTAINMENT



52% (541)

PARKING



49% (509)

PARKS & OPEN SPACE



42% (436)

FOOD & BEVERAGE



40% (408)

CULTURAL



37% (381)

TRAILS



25% (261)

OTHER

***Specified Responses Included:
 "Recreation like YMCA," "Art galleries, unique shops, and antique shops," "Village Green," "Small shops," "Town Square," "Grocery stores," "No more housing," "Waterfront uses"

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

12. WHAT KIND OF DEVELOPMENT WOULD YOU LIKE TO SEE ALONG THE PECONIC RIVER, IF ANY?

Answers: 1,022 Slipped: 217

PARKS & OPEN SPACE TRAILS



58% (602)



52% (541)

MARINA



49% (509)

RETAIL



42% (436)

CULTURAL



40% (408)

NO DEVELOPMENT



37% (381)

CIVIC



25% (261)

OTHER

***Specified Responses Included:
 "Parking," "Permanent indoor farmers market," "No parking on the river," "Keep it natural," "Recreational activities like canoeing and paddle boats," "Public space," "Community or recreational center"

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

13. SHOULD THE CONNECTION BETWEEN MAIN STREET AND THE PECONIC RIVER BE PRIORITIZED?

Answers: 425 Skipped: 427



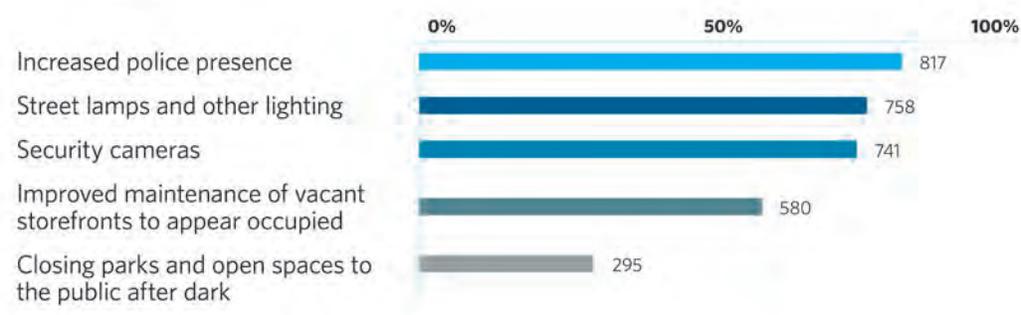
The average percentage of respondents' ranking was 78%

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

14. TO IMPROVE SAFETY AND SECURITY, WHICH OF THESE SOLUTIONS SHOULD BE ADOPTED?

Answers: 1,479 Skipped: 1,023



***Specified Responses Included: "increased foot patrols of police officers," "Social services to address underlying issues," "Clean up the alleyways and the sidewalk areas," "Security staff in the parks, then closed at 10PM," "Attract more foot traffic... there is safety in numbers," "Add a blue-light system to call 911," "Do not increase police presence unless trained in de-escalation," "Improve lighting in all commercial areas," "Do not gate the alleyways," etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

15. HOW DO YOU PERCEIVE THE SAFETY OF MAIN STREET AS A PEDESTRIAN?

Answered: 252 Skipped: 267



The average percentage of respondents' ranking was 36%

DOWNTOWN RIVERHEAD PATTERN BOOK / RIVERHEAD, NEW YORK / MARCH 2020

URBAN DESIGN ASSOCIATES

16. HOW DO YOU PERCEIVE THE SAFETY OF MAIN STREET ON A BICYCLE?

Answered: 294 Skipped: 455



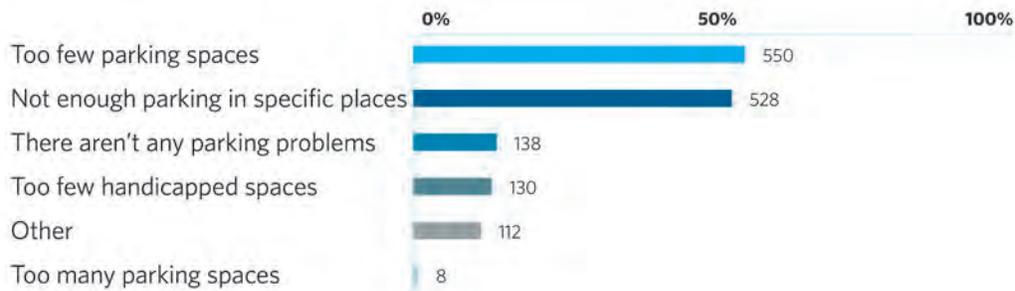
The average percentage of respondents' ranking was 26%

DOWNTOWN RIVERHEAD PATTERN BOOK / RIVERHEAD, NEW YORK / MARCH 2020

URBAN DESIGN ASSOCIATES

17. DOES DOWNTOWN RIVERHEAD HAVE ANY ISSUES RELATED TO PARKING? WHAT PROBLEMS EXIST?

Answers: 107 Skipped: 124



***Specified Responses Included: "Not enough parking in high-volume areas," "The parking that is available floods," "Parking seems adequate except during large events like street fairs," "Parking along the river should go," "Not an issue now, but it will be, once more apartments open," "Need to make sure future parking needs don't threaten historic buildings," "The prettiest area of Riverhead should not be used as a parking lot," "Better public transit to lessen the need for more parking," "Plenty of parking—poorly utilized," "Need a three-story parking garage in part of the lot north of Main Street," etc.

DOWNTOWN RIVERHEAD PATTERN BOOK RIVERHEAD, NEW YORK / MARCH 2020

URBAN DESIGN ASSOCIATES

18. HOW CONCERNED ARE YOU ABOUT FLOODING IN DOWNTOWN RIVERHEAD?

Answers: 107 Skipped: 143



The average percentage of respondents' ranking was 74%

DOWNTOWN RIVERHEAD PATTERN BOOK RIVERHEAD, NEW YORK / MARCH 2020

URBAN DESIGN ASSOCIATES

19. HOW IMPORTANT IS THE REGULATION OF ARCHITECTURAL STYLE IN DOWNTOWN RIVERHEAD?

Response: 100% | Question: 100%



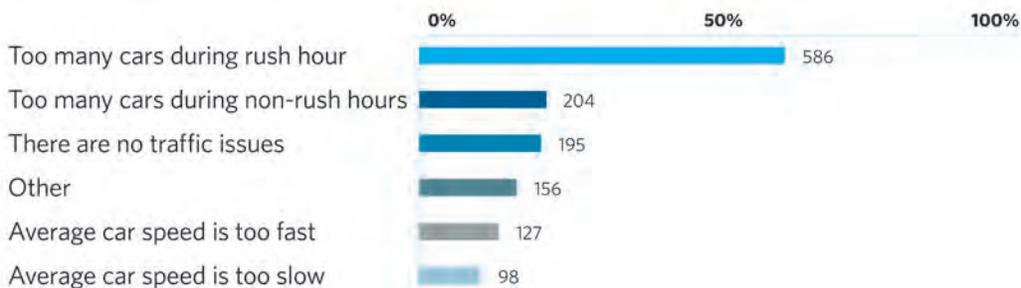
The average percentage of respondents' ranking was 75%

DOWNTOWN RIVERHEAD PATTERNS BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

20. IS TRAFFIC AN ISSUE ON MAIN STREET? IF SO, WHAT SPECIFIC PROBLEMS EXIST?

Response: 76% | Question: 100%

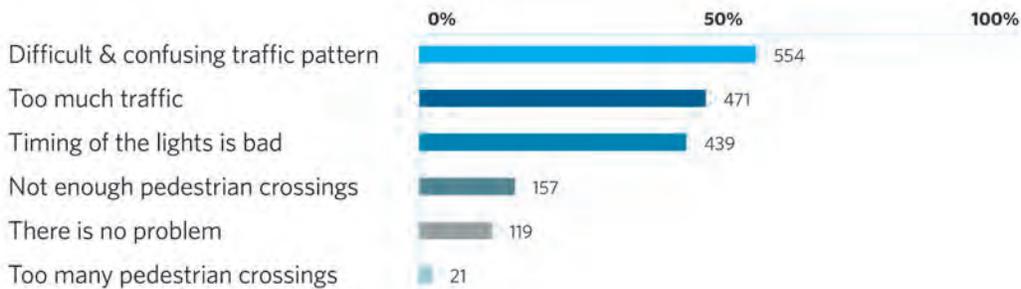


***Specified Responses included: "Traffic lights need reworking," "The Peconic Avenue and Main Street intersection is the problem," "Pedestrian safety isn't even on the radar," "When crossing guards are used to handle the rush hour at Peconic and Raanoke, traffic flows great," "It's hard to turn left going west on Main Street onto Peconic Avenue," "Fast-moving large trucks are a danger," "Main Street should not be open during community events such as Halloween," "Terrible visibility," "Traffic is seasonal, in the summer when tourists come to the aquarium, traffic clogs up," etc.

DOWNTOWN RIVERHEAD PATTERNS BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

21. DOES THE INTERSECTION OF MAIN STREET, PECONIC AVENUE, AND ROANOKE AVENUE HAVE ANY ISSUES?

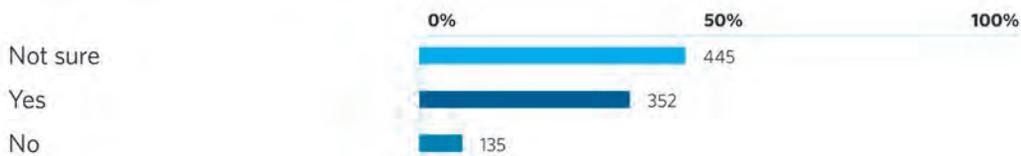


***Specified Responses Included: "The roads don't meet up but I don't think you can fix that." "Police officers directing traffic during rush hours is helpful." "Lights should have traffic cameras." "It's dangerous for pedestrians to use crosswalks." "Shouldn't allow right turns onto Main Street from Peconic during red lights." "It's busy, but that is typical of any vibrant downtown." "Takes forever to get through this area." etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

22. DOES DOWNTOWN RIVERHEAD HAVE A PROBLEM WITH OVERCROWDED HOUSING?



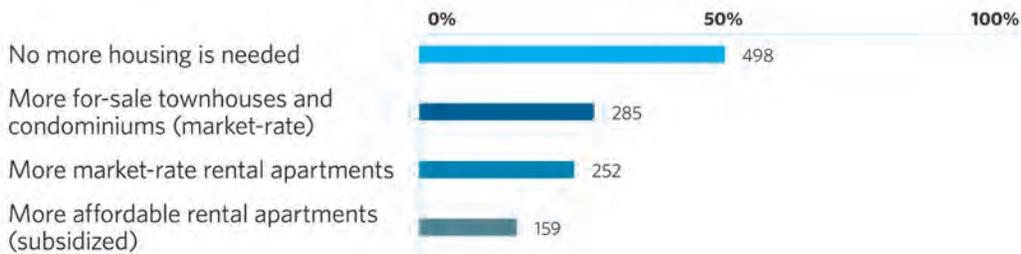
***Specified Responses Included: "Not a problem in downtown, but definitely north of Main Street on 2nd Street and beyond." "Lack of code enforcement and underlying racism causes a perception of overcrowded housing." "Too many slim lots allow it." "The overcrowding is due to landlords subdividing their property." "Gentrification of the area should not be the solution to the overcrowding or illegal housing situations." "Contributes to overcrowded schools." "On side streets the issue is plain to see." etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

23. DOES DOWNTOWN RIVERHEAD NEED MORE HOUSING? IF SO, WHAT TYPE OF HOUSING IS NEEDED?

Responses: 176 | Issues: 171



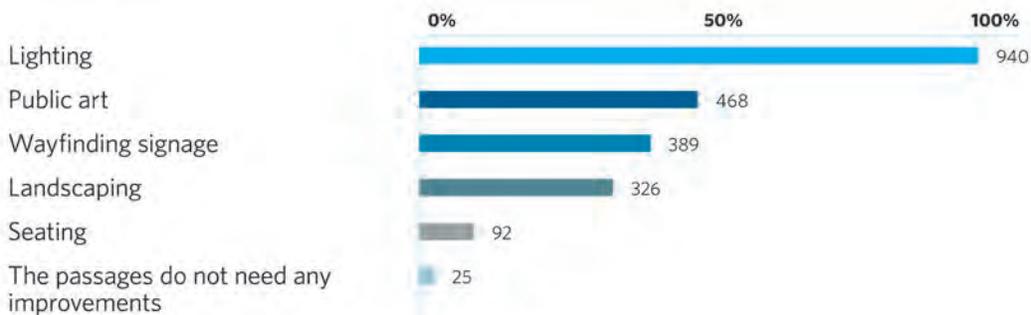
***Specified Responses Included: "Perhaps it is needed, but no more tall buildings," "Decrease subsidized housing," "Well-landscaped condos would be great compared to awful giant monoliths," "More affordable housing for SS," "Housing in the DC-1 area is needed, but not on Main Street," "If the premise is more housing equals footfall equals support for retail/food&beverage, we need market-rate," "Senior housing," "The 500-unit cap should be lifted," "A sustainable mix of housing types with parking mandates is needed," etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2022

URBAN DESIGN ASSOCIATES

24. DO THE PASSAGES/ALLEYWAYS NEED IMPROVEMENTS? IF SO, WHAT TYPE OF IMPROVEMENTS?

Responses: 1305 | Issues: 1347



***Specified Responses Included: "Security cameras," "Monitoring the passages is important," "Should encourage shops in buildings adjacent to alleys," "Clean up areas around trees and signs," "Window displays into stores," "Adding lights on the sides would make it feel safer, especially at night. Adding artwork on the walls would be more welcoming and warming too," "Don't dump here," "Please keep them open for access," etc.

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2022

URBAN DESIGN ASSOCIATES

25. SHOULD RIVERHEAD HOLD MORE, LESS, OR THE SAME NUMBER OF SPECIAL EVENTS?

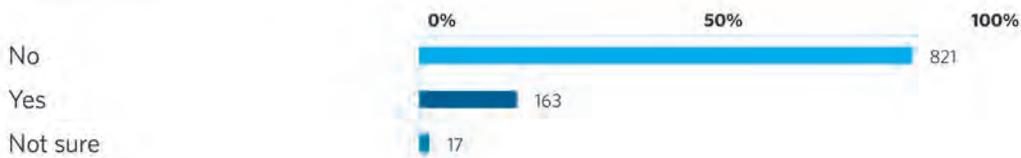


The average percentage of respondents' ranking was 78%

URBAN DESIGN ASSOCIATES

URBAN DESIGN ASSOCIATES

26. HAVE YOU ATTENDED A PUBLIC ENGAGEMENT SESSION OR STAKEHOLDER MEETING FOR THIS PROJECT?



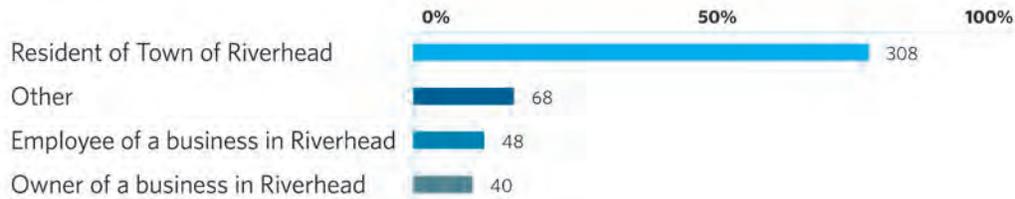
URBAN DESIGN ASSOCIATES

URBAN DESIGN ASSOCIATES

APPENDIX D: ONLINE SURVEYS

SURVEY 2: RESULTS

1. WHICH OF THE FOLLOWING BEST DESCRIBES YOUR RELATIONSHIP TO DOWNTOWN RIVERHEAD?

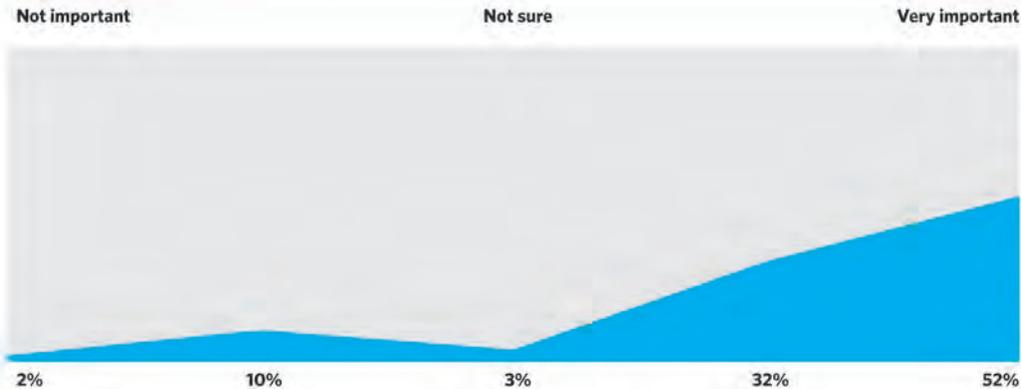


***Specified Responses Included: "Frequent visitor," "Frequent shopper and diner in Riverhead," "Quogue resident," "Summer resident of Quogue," "Go to restaurants and stores on Rt.58," "Resident of Flanders," "Resident of Southold," "Live in Hampton Bays," "I live in Calvert"

DOWNTOWN RIVERHEAD PATTERN BOOK | BROADWAY | PECONIC | JULY 2020

URBAN DESIGN ASSOCIATES

2. HOW IMPORTANT IS IT TO ADDRESS THE DOWNTOWN FLOOD RISK FROM THE PECONIC RIVER?



Over 80% of respondents rated this issue as "Important" or "Very important"

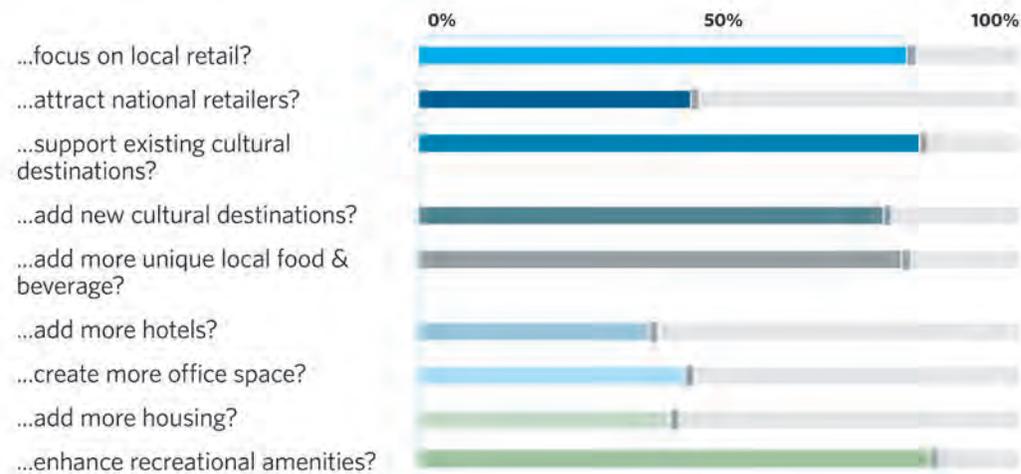
DOWNTOWN RIVERHEAD PATTERN BOOK | BROADWAY | PECONIC | JULY 2020

URBAN DESIGN ASSOCIATES

3. HOW IMPORTANT IS IT TO...



4. HOW IMPORTANT IS IT TO...



5. ARE THERE ANY OTHER USES THAT SHOULD BE CONSIDERED FOR DOWNTOWN RIVERHEAD?

SMALL RETAIL

- Health and wellness businesses
- Coffee shops, social spaces
- Focus on minority-owned businesses
- Retail with personality
- Small shops, boutiques, and cafes to attract tourists
- Restaurants with outdoor seating
- Grocery store

REC & EVENTS

- Trails, picnic areas, benches
- Focus on Peconic River
- Open area for outdoor concerts with seating
- Upscale landscaping in open area
- Walkable downtown
- Community space for kids
- Ice skating rink in winter
- Open air markets

CULTURAL

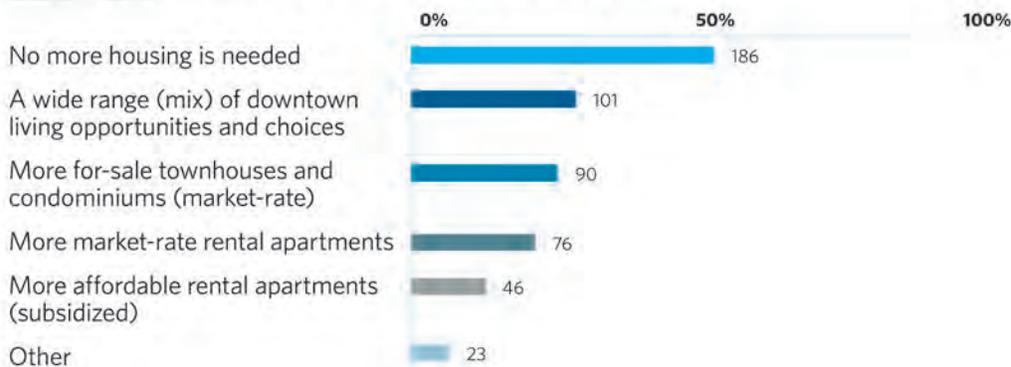
- Museums and art galleries
- Amphitheater for events
- Cinema
- Maker spaces

OTHER

- No more non-affordable housing
- No more big buildings
- Needs to be more walkable
- Needs better public transit
- More bicycle lanes
- More parking needed in specific locations
- Fill vacant buildings and lots

URBAN DESIGN ASSOCIATES

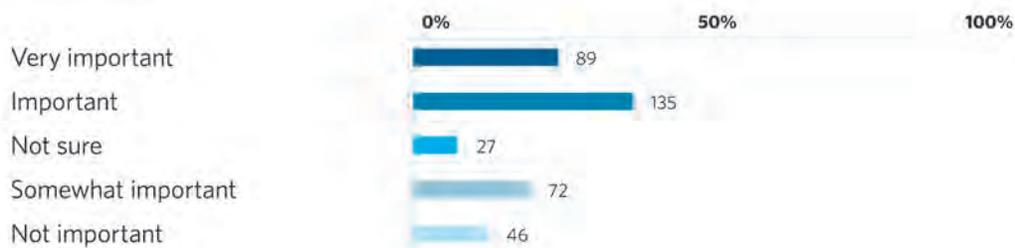
6. WHAT TYPE(S) OF HOUSING DO YOU THINK DOWNTOWN RIVERHEAD NEEDS, IF ANY?



***Specific Responses Included: "Town of Riverhead needs more housing, but not on Main St." "Schools are overcrowded and lack of parking means more housing undesirable." "Unclear about definition of affordability."

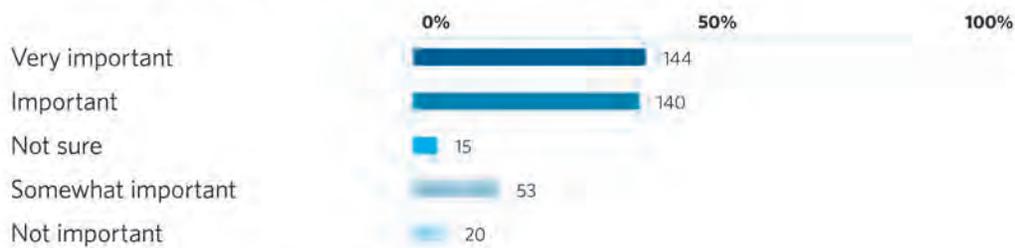
URBAN DESIGN ASSOCIATES

7. HOW IMPORTANT IS IT TO CALM THE SPEED OF TRAFFIC ON E MAIN STREET?



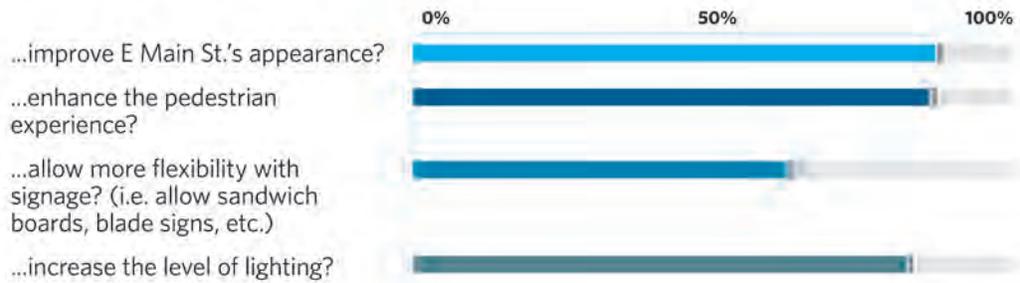
About 60% of respondents rated this issue as "Important" or "Very important"

8. HOW IMPORTANT IS IT TO IMPLEMENT TRAFFIC CALMING MEASURES TO IMPROVE THE SAFETY OF E MAIN STREET'S PEDESTRIAN CROSSINGS?

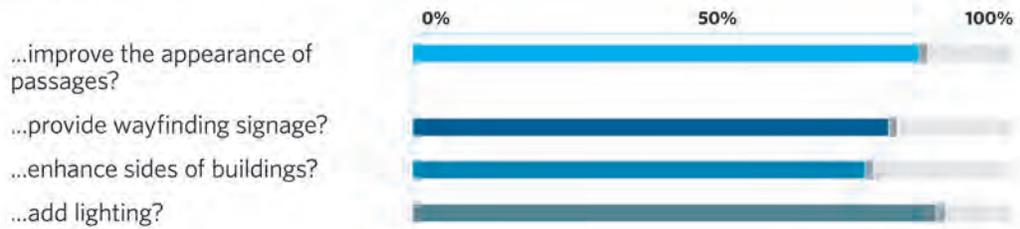


Over 75% of respondents rated this issue as "Important" or "Very important"

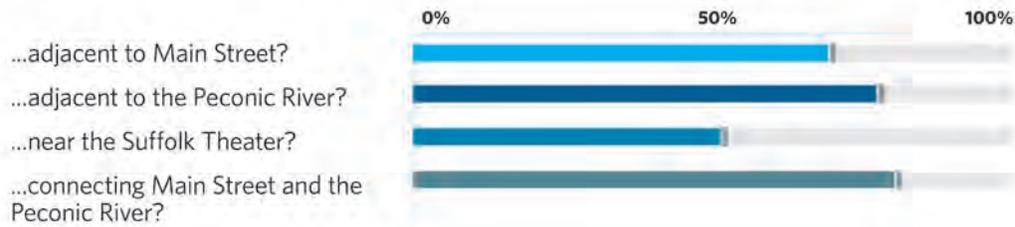
9. HOW IMPORTANT IS IT TO...



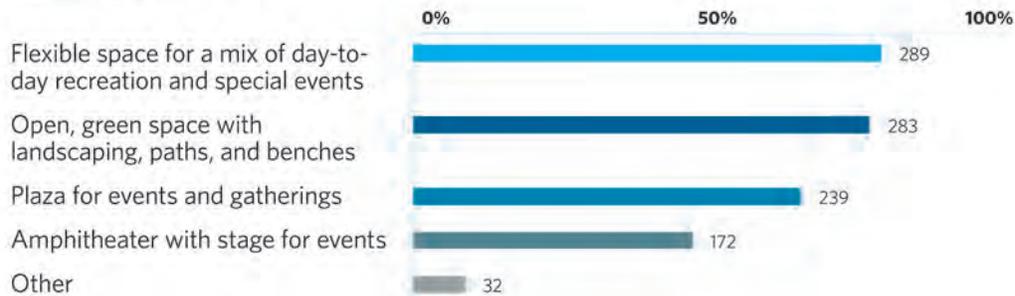
10. HOW IMPORTANT IS IT TO...



11. HOW IMPORTANT IS IT TO LOCATE A TOWN SQUARE...

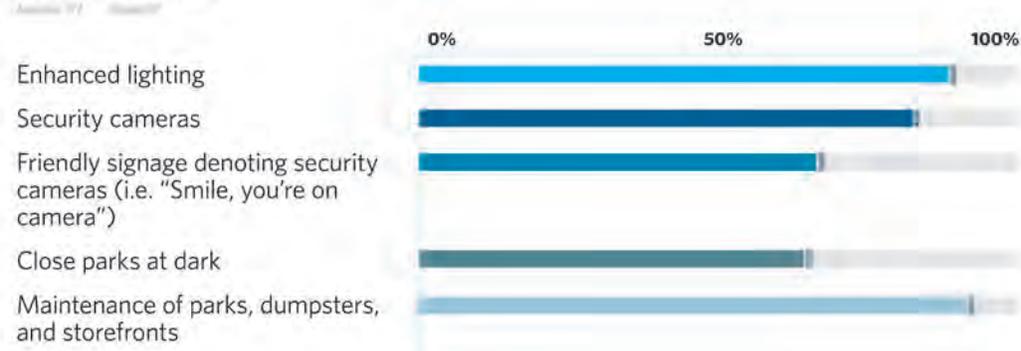


12. WHAT KIND OF EXPERIENCE SHOULD A TOWN SQUARE OFFER?



***Specified Responses Included: "Food trucks or pavilion," "Mix of benches and open area," "Playground for kids," "Pop-up market spaces," "Fountain or splash pad," "Ice skating rink in the winter," "Carousel or ice rink to draw families," "Seasonal retail cart kiosks," "Bike racks and access to transit," "Hydration stations," "Covered picnic tables"

13. WHAT TECHNIQUES ARE MOST IMPORTANT TO INCREASE THE SAFETY OF PUBLIC SPACE IN DOWNTOWN RIVERHEAD?



14. IN TERMS OF LOT COVERAGE AND BUILDING HEIGHT, WHICH ALTERNATIVE IS YOUR FAVORITE?



15. IN TERMS OF LOT COVERAGE AND BUILDING HEIGHT, WHICH ALTERNATIVE IS YOUR LEAST FAVORITE?

Accession ID: 297 Street ID: 10

ALTERNATIVE A

80% Lot Coverage, 60 ft. Bldg Height, 5 Stories, 4.0 FAR



75% (272)

ALTERNATIVE B

100% Lot Coverage, 50 ft. Bldg Height, 4 Stories, 3.5 FAR



12% (44)

ALTERNATIVE C

80% Lot Coverage, 50 ft. Bldg Height, 4 Stories, 2.75 FAR



2% (5)

ALTERNATIVE D

75% Lot Coverage, 40 ft. Bldg Height, 3 Stories, 2.0 FAR



11% (38)

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | JULY 2020

URBAN DESIGN ASSOCIATES

16. IF A BUILDING IS TALLER THAN 3 STORIES, WOULD YOU PREFER A SETBACK ON THE TOP FLOOR(S)?

Accession ID: 297 Street ID: 10

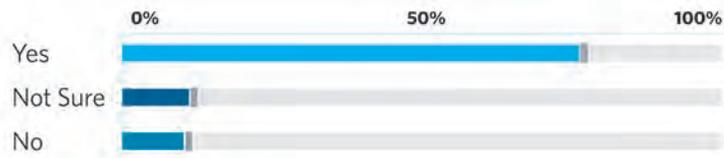


DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | JULY 2020

URBAN DESIGN ASSOCIATES

17. DO YOU THINK THE ARCHITECTURAL STYLE OF NEW BUILDINGS IN DOWNTOWN RIVERHEAD SHOULD BE REGULATED?

November 2019 | Survey # 70

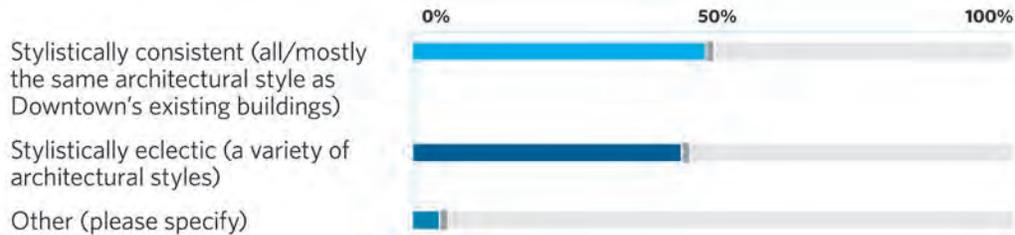


DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

18. WOULD YOU RATHER NEW BUILDINGS IN DOWNTOWN RIVERHEAD APPEAR:

November 2019 | Survey # 71



***Specified Responses (included): "Eclectic with some sort of theme," "Consistent with original buildings from the 1800s-1900s; definitely pre-WW2," "Historical where possible," "Model Riverhead after Greenport, not Patchogue," "Currently there isn't a single architectural style for downtown," "Quaint old village look," "They should exude charm and fit in with what is there," "Upscale materials, not industrial," "Similar styles, or consistent look," "Brighter coloring," "Hampton's stylish North Fork"

DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

19. WHAT ARCHITECTURAL STYLE CONCEPT WOULD YOU PREFER FOR NEWLY-CONSTRUCTED BUILDINGS IN DOWNTOWN RIVERHEAD?

Answers: 135 Skipped: 34



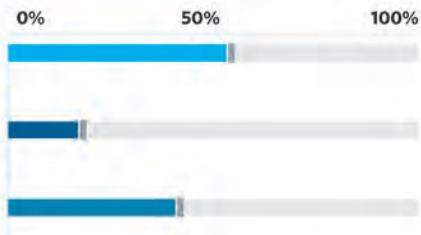
References Pre-WWII styles



References Post-WWII styles



Uses modern-day/contemporary styles

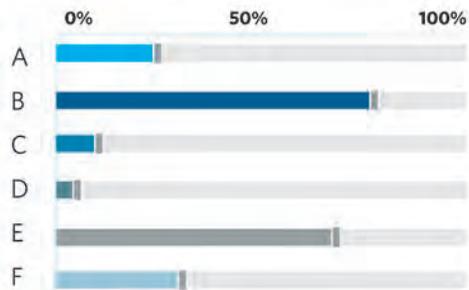


DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

20. OF THE 6 DOWNTOWN RIVERHEAD BUILDINGS SHOWN, WHICH THREE BEST EXPRESS THE SPIRIT OF RIVERHEAD, IN TERMS OF ARCHITECTURAL CHARACTER?

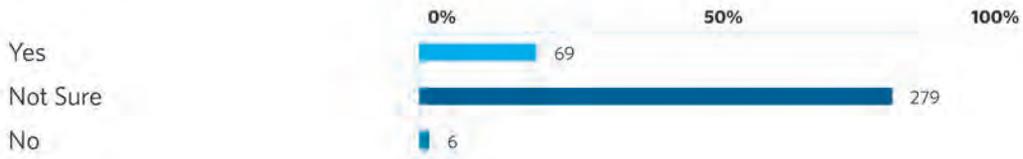
Answers: 263 Skipped: 42



DOWNTOWN RIVERHEAD PATTERN BOOK | RIVERHEAD, NEW YORK | MARCH 2020

URBAN DESIGN ASSOCIATES

21. HAVE YOU ATTENDED A PUBLIC ENGAGEMENT SESSION OR STAKEHOLDER MEETING FOR THIS PROJECT?



© 2018 Urban Design Associates, LLC

URBAN DESIGN ASSOCIATES

This page is intentionally blank.

APPENDIX E: RANKING CHALLENGES

Listening to residents' impressions of Downtown Riverhead informs the direction of further community engagement and decision-making.

The first step of any community-led design process is establishing common ground. Hosting multiple meetings amongst major stakeholders and community members, reviewing the upcoming engagement processes, and listening to residents' lived experiences of a place is a necessary step in building consensus. The dot exercise is one exercise that quickly communicates a community's impression of their town in a visual medium. In each public and stakeholder meeting, the community was asked to mark the strengths, weaknesses, opportunities, and threats in the downtown DC-1 zoning district using green, red, blue, and yellow stickers, respectively. Combining all these stickers digitally starts to paint a picture of the community's impression of Downtown Riverhead.

PARKING (2-14-2020)



Parking

Per the Riverhead Parking Plan recommendations:

- **Prioritize spaces for customers and visitors in centrally located facilities.**
- **Update enforcement efforts to better monitor and manage parking demand.**
- **Establish regulations for incoming developments to prepare for increased residential parking demand.** This would include requiring that all new residential construction downtown provide adequate on-site parking for residents. Alternatively, developers should have the option to pay for their pro rata share of a new downtown parking structure.
- **Enhance alternative transportation amenities to reduce parking demand.**

Note

Current peak demand does not exceed 58%. Meaning, that constructing additional parking at this time is not necessary. Parking garages north and south of Main street should be considered when demand warrants.



I agree with these recommendations.



Downtown does not have a parking problem and therefore no changes are necessary.



- Covered parking for new housing
- Better maintain + control current parking to increase positive perception
- Metered parking; more trees in the sea of asphalt!
- Need more disabled parking
- Solar covered parking stalls
- Handicap parking
- Southern parking structure not in good location, blocks views
- Equalize dispersion of parking — 60% of parking (or more) is on the west end of Main Street, while high volume areas like by the aquarium are low-volume
- Have metered parking; smaller spaces

★ POTENTIAL FOR FUTURE PARKING GARAGE LOCATION



PROPOSED MIX OF LAND USES (2-14-2020)

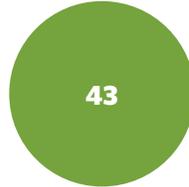


Proposed Mix of Land Uses

Suggestion

Focus on supporting and adding uses that C.R.58 and other regional downtowns don't have:

- Existing cultural destinations (LI Science Center, Suffolk Theater, SCCC Culinary Arts, East End Arts, Vail-Leavitt Music Hall)
- New cultural destinations (museum collections, evening entertainment, arts & music, etc.)
- Additional unique local food and beverage
- Ancillary local retail
- Additional hospitality
- Postsecondary education
- Upper story office
- Wide range of downtown living opportunities and choices



I agree that this list of uses should be prioritized

● 3

I would like to see nation chains downtown, similar to C.R.58



- We need smaller natural chains; Starbucks, Trader Joe's, etc.
- Small specialty stores; gourmet food; cheese wine liquor; Uncle Giuseppe's
- Keep it hometown like; Sayville more, specialty shops
- Just Trader Joe's as a large store
- Movie theater N side of Main
- Indoor pickleball; indoor recreation
- Tax breaks for boutiques and small retail stores
- Need movie theater
- More public art



East End Arts



The Suffolk Theater



Starr Confectionery



Restaurant (red) and entertainment (purple) heat map



The existing mix of uses is focused largely on commercial, including restaurants and offices

HOUSING (2-14-2020)



Housing

Question
What type of new housing does Downtown Riverhead need?



- Until current new housing is filled a moratorium should be put on new projects until their impact can be assessed
- Create more jobs then consider housing
- Housing outside of "boundary" — east of Elton? West of Library?
- Should have a lot of work/live units
- Retiree friendly market rate apartments
- Need to avoid gentrification of downtown

More affordable rental apartments (subsidized)



10 ● 9

More for-sale townhouses and condominiums (market-rate)



13 ● 4

More market rate rental apartments



14 ● 4

A wide range (mix) of downtown living opportunities and choices



17 ● 1

Downtown does not need more housing

X 7 ● 6

VEHICULAR CIRCULATION (2-14-2020)



Vehicular Circulation

Suggestion

Major intersection improvements would require the demolition of occupied building—something that is politically and economically improbable. The existing street layout is therefore likely to remain. However, the town should work with the State and the Police Department on ways to calm the speed of traffic on Main Street.

19

16

I agree that the Town should work with the State and the Police Department on ways to calm the speed of traffic on Main Street.

The speed of traffic is fine on Main Street and should not be further restricted.



- Parking should be abolished in Town. It could be accommodated at Grumman with a metro-rail to town, solar panels over the parking and pedicabs in town
- In time, add a golf cart or small train to transport visitors + shoppers
- Enforce speed limit
- Dutch model? Level surface for all; different materials to delineate pedestrian, bike, auto
- People (drivers) don't adhere to rules such as "blocking the box" @ Roanoke
- 25 mph just like incorporated village of South Hampton
- East Bond main st traffic should be no turn on red to Peconic Ave.
- Peconic Ave should be one way only north
- Creating a welcoming + comfortable experience downtown requires treating it as the destination it is. Vehicles need to slow down <25 mph
- Traffic signal sometimes does not seem timed properly for vehicles going west on Main St. + turning left on Peconic Avenue backs up!
- West Main St beyond downtown need more red lights, policing and attention. 2 many people (pedxing) getting killed
- Need 'Don't block the box' at parking lot exits
- Make Ostrander, Union, Maple, East Ave one way
- Can't see lowered traffic light at Peconic/Roanoke intersection

- PRIMARY STREETS
- SECONDARY STREETS
- ALLEYS



PEDESTRIAN CIRCULATION (2-14-2020)



36

8

Pedestrian Circulation

Suggestion

Work with the State to add pedestrian actuated flashing lights at all unsignalized crossings

- Add curb bump-outs into the parking lane at mid-block crossings to increase pedestrian visibility and safety
- Add a mid-block crossing at Union Avenue to reduce the distance between crosswalks
- Consider replacing striping in the parking lane with curb bump-outs
- Consider adding temporary and/or permanent parking space parks

I support trying to improve the Main Street pedestrian crossings

The Main Street crosswalks work fine and should not be modified



- Stop for pedestrians sign would help
- Irrigation or maintenance + small fence for flowers
- Lit crosswalks with push of button flashing
- Every tree should touch the next to form a canopy!
- Handicap accessible wheelchairs
- Pedestrian street type passage along N side of parking lot on river
- Put visible signs embedded in street esp. for a night
- Bump light in the street crosswalk
- Pedestrian bridges across Main St.
- Police presence in crosswalks
- Bump-outs are confusing + cause problems



Existing pedestrian infrastructure consists primarily of sidewalks on streets and passages between buildings

- SIDEWALKS
- PASSAGES



Using extra space on Main Street to buffer pedestrians from car traffic will improve safety

- SIDEWALKS
- PASSAGES
- CURB BUMP-OUT
- ON-STREET PARKING
- CROSSWALK
- PROPOSED CURB BUMP-OUT
- PROPOSED CROSSWALK

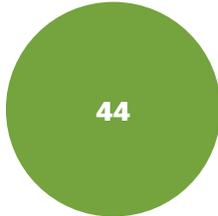


Example of a curb bump-out



Plan view of a curb bump-out

PEDESTRIAN-FRIENDLY STREETS (2-14-2020)



• 1



Pedestrian-Friendly Streets

Suggestion

Consider beautifying the “amenity” and “marketing” zones to make them more inviting. Improvements could include:

- Amenity Zone:
 - Additional landscaping
 - Planters
 - Public art
 - Seating
 - Seat walls
 - Bike racks
 - Decorative lighting
 - Wayfinding signage
 - Special paving
 - Temporary and/or permanent parking space parks
- Marketing Zone:
 - Storefront beautification (extension of window displays to exterior spaces)
 - Awnings
 - Decorative lighting
 - Outdoor seating
 - Planters
 - Blade signs
 - Sandwich Boards

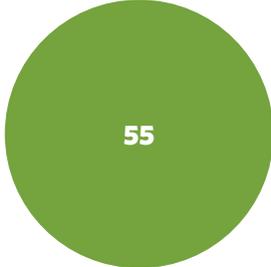
I agree that the Town and businesses should work together to improve the appearance of Main Street

The pedestrian experience along Main Street is fine and should not be modified

- Tree canopy on Main
- Empty store fronts could have adverts from local nonprofits, social groups, educational groups until occupied
- Sidewalks should be same width
- Unified awnings; unified signage
- Police presence in crosswalks



PEDESTRIAN-FRIENDLY PASSAGES (2-14-2020)



Pedestrian-Friendly Passages

Suggestion

Consider animating passages to make them more inviting with a mix of:

- Gateways
- Additional decorative lighting
- Public art
- Wayfinding signage
- Building entrances
- Outdoor seating
- Landscaping
- Landscape elements

I agree that this list of improvements should be prioritized

● 5

The passages are fine the way they are with the exception of simply adding more lighting for safety



- Public Art
- Nautical themed signage
- Historical Street lamps
- Landscape maintenance
- Lighting but not cheap garage LED's too much light is worse than none
- Decorative lighting + landscaping
- Display windows
- Lighting does not equal security necessarily
- Graphics are confusing!
- Cameras



Wayfinding signage



Landscaping



Landscape elements



Additional decorative lighting



Public art



Building entrance

PARKS AND OPEN SPACE (2-14-2020)

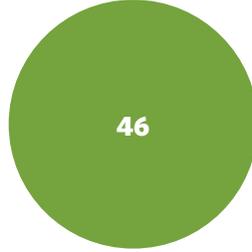


Parks and Open Space

Riverhead was just awarded an \$800,000 state grant to help establish the "town square."

Suggestion

Create a town square (flexible multipurpose plaza) along Main Street as a permanent central gathering place and link it to the Peconic riverfront with a significant open space gesture. Explore opportunities for secondary gathering places and space to stage temporary events.



3

I agree with this idea and would like to see the town square about Main Street

I agree with this idea, but the town square could be on a repurposed riverfront parking lot



- Town Square could be used as market on weekends; should have cafes or food trucks; fixed + flexible seating
- The river has been left out of these proposals. A water feature like a fountain in the river on axis with the Suffolk Theater would set up an axis that would give focus to the Town
- Go for art program
- Needs activities; band shell; vendors; art; something for boaters
- More trees on Main + in the sea of asphalt
- Farmers Market; Renovate top and make observation deck; Breezeway/Open Air
- Indoor playground; outdoor playground; splash pad



The institutional uses and open spaces within Downtown represent both local and regional draws



The town square has been discussed in proximity to the vacant Sweezy's building



Existing riverfront park



Visions for a town square have been discussed for years

PUBLIC SAFETY (2-14-2020)

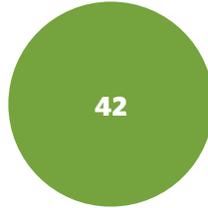


Public Safety

Suggestion

Consider implementing Crime Prevention Through Environmental Design (CPTED) principles to improve public safety:

- Improved natural surveillance
 - Well-lit building entrances, pedestrian areas, and parking lots
 - Eliminate hiding spots
 - Unobstructed sightlines
 - Security cameras
 - Closing parks and open space after dark
- Natural access control
 - Use curbing and landscaping to direct automobile and foot traffic into controlled, visible areas.
- Territorial reinforcement
 - Clear delineation between public and private property
 - Prioritize places to socialize, but not loiter
 - Establish relationships that promote eyes on streets and parks
- Maintenance
 - Well-maintained public areas and storefronts sends the message that people care about their downtown. This in turn discourages vandalism. Security practitioners refer to the "Broken Windows Theory," the idea that one broken window will entice vandals to break another. Elements like open dumpsters are the equivalent of a broken window.



I agree that this list of security enhancements should be prioritized

Public safety is a perception issue that can be resolved with greater education and awareness. I therefore disagree with making physical changes.



- I think education + awareness is a key item
- Graffiti free
- Paint Alley-ways Floors green like grass; paint walls with murals
- Lighting but not cheap LEDs
- Police substation? Windows in alleyways
- Activate the streets!
 - Activate vacant storefronts even by non-traditional measures
 - Outside seating
 - Outdoor events
 - Mixed-use buildings
 - Late night businesses
 - Pedestrian scale lighting
 - Enhance eyes on the street



Minimal lighting



Freestanding dumpster and storage container



Vacant storefront window



Stacked pallets

CHARACTER OF DEVELOPMENT (2-14-2020)



Character of Development

Suggestion

Require new buildings to fit within their context versus prescribing a particular architectural style(s). Existing downtown buildings reflect the period in which they were built. They are contemporary in the sense that they are of their time. This eclectic mix gives downtown its unique character. However, new buildings should maintain a common vocabulary (scale, massing, articulation, materials, etc.) with neighbors — especially historic structures.

28

I agree with this eclectic approach

13

I disagree with this approach. The character of new buildings should match one of the existing architectural styles along Main Street.



- Don't like new construction style — too big
- Buildings should not be 5 stories; 3 to 4 story buildings should have architectural details to make them blend with existing buildings
- Do not tear down historic structures! Agree!
- Buildings adjacent to historic structures should have to respect their integrity
- It's not too late! The building of the current 5 story monstrosity should be required to add details to relieve the "box" look

>1900



Development in the 19th century included churches, large single-family houses, and relatively tall mixed-use buildings around Downtown Riverhead's main intersection.

1900-1950



Between 1900 and 1950, development was focused on two-story retail buildings with large storefront windows and minimal setbacks, creating a commercial Main Street.

1950-2000



The second half of the 20th century saw major changes in retail: small, local storefronts were traded for large department stores with larger footprints and increased parking requirements.

2000-Present



The past 20 years of development consisted mostly of multi-family apartment buildings and institutional anchors, including the Long Island Aquarium and the Suffolk County Culinary Arts School.



LOT COVERAGE/BUILDING HEIGHTS (2-14-2020)



POLICY



PUBLIC



DEVELOPMENT

Lot Coverage/Building Heights

Put a green dot your most favorite
Put a red dot on your least favorite



Resident Comments from the February 19 Community Meeting

- Require front type facade facing river
- Trees in the sea of asphalt parking lots

Alternative A (No Change)

- 80% Lot Coverage/60 ft. Building Height/5 Stories/4.0 FAR



1 . **26**

Alternative B

- 100% Lot Coverage/50 ft. Building Height/4 Stories/3.5 FAR



20 . 2

Alternative C

- 80% Lot Coverage/50 ft. Building Height/4 Stories/2.75 FAR



12 . 3

Alternative D

- 75% Lot Coverage/40 ft. Building Height/3 Stories/2.0 FAR



15 . 5

VERTICAL SETBACKS (2-14-2020)



43

7



Vertical Setbacks

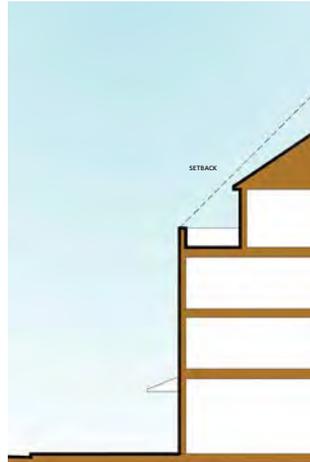
Suggestion

Require 45 degree setback above 3rd floor along streets and passages. Also, 45 degree setback from the eave of freestanding historic structures. Dormers, cupolas, chimneys, and parapets may encroach into the 45 degree setback.

I agree with requiring a setback above three stories

I think a setback should be up to the developer and architect

- Agree w/ setbacks but want height limits to 3 stories
- 3 stories with setbacks that don't impact street
- Setback OK but NO 5 stories!!
- Setback, 45 degree, should go back from building line all the way to top floor, on south side of Main St. to cut down on fall to spring shadows
- Yes to setbacks so we can have large trees!



HORIZONTAL SETBACKS (2-14-2020)



Horizontal Setbacks

Suggestion

Match the setback of adjacent buildings except when next to freestanding historic structures. Require 45 degree setback from freestanding historic structures to promote visibility.



● 4

I agree with requiring a special setback from historic structures

This seems unfair — no special setbacks should be required



- Building set back to adjacent historic bldgs; no buildings more than 1 story
- Needs texture along facade; urban furniture should be movable
- Leave the space east of historic buildings vacant for a town sq



APPENDIX F: REFERENCES & RESOURCES

Previous studies and planning efforts were gathered and incorporated into this set of recommendations.

The collection and consolidation of any existing planning efforts, studies, and proposals was a crucial part of this effort. Between inconsistent development patterns, economic and environmental fluctuations, and the changes in neighboring areas, Downtown Riverhead has faced complicated policy issues and has made many attempts to establish a status quo. In the last three decades, both comprehensive and highly specific projects have been implemented with varying success, but the sheer quantity of planning efforts often leads to redundancies, contradictions, and confusion.

This Pattern Book and community engagement process were largely informed by the following list of documents. Selected portions of these documents follow this page.

- Riverhead Parking Plan, Preliminary Recommendations (2019)
- Project Opportunities Map (2019)
- Town of Riverhead Peconic River/Route 25 Corridor BOA Study (2016)
- TOD Growth Plan for the Town of Riverhead Peconic River/NYS Route 25 Corridor - BOA Step II Nomination (2015)
- Zoning Use Districts Map (2015)
- Market Analysis: Community Profile (2015)
- Parking Districts Map (2015)
- Downtown Riverhead Historical District Boundary (2010)
- Railroad Street Corridor Plan (1997)
- Business Improvement District Plan (1995)
- East Main Street Renewal (1993)
- Market Analysis: Business Improvement District (1993)

This page is intentionally blank.

APPENDIX G: RECONNAISSANCE

Studying and recording the features of Downtown Riverhead and other nearby towns through photography and measuring is a crucial step.

Of the many aspects that make Downtown Riverhead special, its location and regional context are certainly near the top of the list. Sited at a crucial junction between the North and South Forks of Long Island, Riverhead is surrounded by quaint hamlets, iconic historical architecture, and touristy business districts. Understanding the successes and weaknesses around the East End helps to identify the opportunities in Downtown Riverhead.

Over four thousand photos were taken and organized by the consultant team throughout the Town of Riverhead and seven other East End towns:

- Babylon
- Greenport
- Huntington
- Northport
- Patchogue
- Sag Harbor
- Southampton

Areas of focus in each town correspond to features typically studied, referenced, recommended, and prescribed in pattern books. The following pages represent only some of the photos taken.



PHOTO SUBJECTS

- Architectural Details (chimneys, columns, doors, eaves, foundations, storefront lighting, materials, murals, parapets, signage, windows)
- Building Massing (balconies, bays, elevations and oblique angles, porches, roofs, stoops, storefronts)
- Parks and Streetscapes (back-of-house, commercial and residential frontage, landscaping, streetscape lighting, open spaces, parking lots and garages, on-street parking, passages, urban furniture like benches, trash cans, parking meters, bollards, etc.)

PHOTOGRAPHS BY TOWN



Babylon



Greenport



Huntington



Northport





Patchogue



Sag Harbor



Southampton

PUBLIC REVIEW DRAFT

This page is intentionally blank.